

**CITY OF ROCHESTER HILLS
BROWNFIELD REDEVELOPMENT AUTHORITY**

BROWNFIELD PLAN

**ROCHESTER/AVALON RETAIL DEVELOPMENT
LOCATED AT 945 AND 975 SOUTH ROCHESTER ROAD
ROCHESTER HILLS, MICHIGAN**

November 2, 2021

Approved by RHBRA:
Approved by City Council:

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PROJECT SUMMARY

Project Name:	Proposed Rochester/Avon Retail Development
Project Location:	The subject property is located at 945 and 975 South Rochester Road in Township three north (T3N), Range eleven east (R11E), Section 14, Rochester Hills, Oakland County Michigan 48307 (the "Property").
Type of Eligible Property:	The subject property is determined to be a "Property" under Part 213 of P.A. 451, as amended and the rules promulgated thereunder, and adjacent and contiguous as defined in Act 381.
Eligible Activities:	Pre-Approved Activities, Department Specific Activities, Asbestos Containing Materials (ACM) Abatement, Demolition, and Preparation of a Brownfield Plan.
Developer Reimbursable Costs:	\$508,553 (includes eligible activities and 15% contingency)
LBRF Capture	\$183,318
Years to Complete Reimbursement :	15 Years from start of capture
Estimated Capital Investment:	Approximately \$10,125,000 million (including Acquisition, Hard and Soft Costs)
Project Overview:	The proposed redevelopment will transform one of the City's busier intersections by demolishing an existing 1,407 square foot former gas station and a 5,409 commercial office building and redeveloping it into a modern, two-story, 26,575 square foot retail and office building. The first floor of the new building will support tenants that include retail, a drive thru restaurant, a credit union, and medical office space. The second floor will be used for office space. It is estimated that the project will create 100 construction jobs and 70 new permanent jobs in the first three years.

I. INTRODUCTION AND PURPOSE

In order to promote the revitalization of environmentally distressed, historic, functionally obsolete and blighted areas within the boundaries of Rochester Hills (“the City”), the City has established the Rochester Hills Brownfield Redevelopment Authority (RHBRA) the “Authority” pursuant to the Brownfield Redevelopment Financing Act, Michigan Public Act 381 of 1996, as amended (“Act 381”).

The purpose of this Brownfield Plan (the “Plan”) is to promote the redevelopment of and investment in the eligible “Brownfield” Property within the City and to facilitate financing of eligible activities at the Brownfield Property. Inclusion of Brownfield Property within any Plan in the City will facilitate financing of eligible activities at eligible properties and will provide tax incentives to eligible taxpayers willing to invest in revitalization of eligible sites, commonly referred to as “Brownfields.” By facilitating redevelopment of the Brownfield Property, this Plan is intended to promote economic growth for the benefit of the residents of the City and all taxing units located within and benefited by the Authority.

The identification or designation of a developer or proposed use for the Brownfield Property that is subject to this Plan shall not be integral to the effectiveness or validity of this Plan. This Plan is intended to apply to the eligible property identified in this Plan and, to identify and authorize the eligible activities to be reimbursed utilizing tax increment revenues. Any change in the proposed developer or proposed use of the eligible property shall not necessitate an amendment to this Plan, affect the application of this Plan to the eligible property, or impair the rights available to the Authority under this Plan.

This Plan is intended to be a living document, which may be modified or amended in accordance with and as necessary to achieve the purposes of Act 381. The applicable sections of Act 381 are noted throughout the Plan for reference purposes.

This Brownfield Plan contains information required by Section 13(2) of Act 381, as amended

II. GENERAL PROVISIONS

A. Description of the Eligible Property (Section 13 (2)(h)) and Project

The Eligible Property consists of three (3) legal parcels totaling approximately 2.199 acres with street addresses of 975 and 945 South Rochester Road, Rochester Hills, Oakland County, Michigan. The parcels and all tangible personal property located thereon will comprise the eligible property and is referred to herein as the “subject property.”

The subject property is located on the Rochester Road corridor, bounded by the commercial use to the north and east, Avon Road to the south, and South Rochester Road to the west. The subject property has been determined be a “Property”.

Property Address	Parcel ID	Approximate Acreage	Eligibility
975 S. Rochester Road	70-15-14-351-012	0.504 acres	Property
945 S. Rochester Road	70-15-14-351-011	0.505 acres	Adjacent and Contiguous
Unaddressed	70-15-14-351-068	1.190 acres	Adjacent and Contiguous

***Brownfield Plan for the Proposed Rochester/Avon Retail Development
Located at 945 and 975 South Rochester Road, Rochester Hills, Michigan
PM Project No. 01-11390-0-0003; November 2, 2021***

Rochester Avon Partners, LLC is the project developer (“Developer”). Rochester Avon Partners, LLC, is managed by Mr. Doriad Markus who has successfully completed numerous commercial retail developments in southeast Michigan. Some of these developments include the Holiday Inn Express in Birmingham that included the redevelopment of a 64-room hotel with several amenities, the Auburn Marketplace in Auburn Hills, a 12,000 square foot commercial retail development, and the Rochester Auburn commercial development in Rochester Hills, which included the development of over 32,000 square feet of commercial retail space over hour buildings. The success of these projects is reflective of Mr. Markus’ ability to attract both national and local tenants in the restaurant and hospitality industry.

The parcels are currently zoned B-1: Local Business District. The subject property is currently occupied by a 1,407 square foot vacant gas station building and a 5,409 square foot office building in an area characterized by commercial uses. Tenants of the office building will be leasing space in the new building. The zoning will remain unchanged as the current zoning permits the proposed future use.

Standard and historical sources document the parcel at 975 S. Rochester Road consisted of agricultural and vacant land from at least 1937 until between 1964 and 1967 when the current building was constructed in the northeastern portion. The building was utilized for gasoline service station operations from at least 1967 until 1999, when the building was renovated into the current convenience store and gasoline dispensing station. A canopy was constructed in the western portion in 1980, which was replaced with the current canopy in 1997. The subject property has operated as a gasoline dispensing station until 2020. The property is currently vacant.

The parcel at 945 S. Rochester Road was developed with agricultural fields from at least 1937 until between 1949 and 1957 when the current commercial building was constructed. Additions to the northern portion of the building were constructed in 1977. Occupants included a veterinarian hospital from at least 1957 until 1975 and various retail businesses and professional offices since the late 1970s. Occupants of the building prior to 1957 were unable to be identified. However, based upon the size and layout of the building during this time period, the building was likely utilized for similar veterinarian hospital operations prior to 1957. The property is currently occupied by three office tenants.

The unaddressed parcel, which is comprised of 1.19 acres and was previously part of a larger 2.33-acre parent parcel that is identified as 923 S. Rochester Road, was developed prior to 1940 with agricultural fields. A dwelling partially located on the subject property and north adjoining property was constructed in the northern portion between 1940 and 1949. Agricultural activities ceased and the former dwelling was renovated and expanded to the Avon Center Hospital in 1954, which extended onto the north adjoining property. Several additions were constructed to the hospital building and a garage structure was constructed in the northeastern portion of the subject property between 1957 and 1963. The former hospital was demolished between 1970 and 1971, and the current paved parking lot was completed. The former garage structure was demolished between 1980 and 1990, and the eastern and southeastern portions have remained vacant land since that time.

The subject property’s legal description is included in Appendix A. A parcel location map is included in Appendix B.

The proposed redevelopment includes demolition of the existing 1,407 square foot former gas station and 5,409 commercial office building to prepare the Property for a larger, two-story,

26,575 square foot commercial building. The project will feature retail, a restaurant, and office spaces on the ground level, with commercial office space on the second floor. Approximately 134 parking spaces will be created, as well as landscape and pathway improvements along the Rochester and Avon Roads that will improve the visual aesthetics of the intersection. The development will activate two currently vacant parcels and will maximize the use of the three parcels, which as a whole are currently underutilized.

Demolition activities are anticipated to begin in the fall of 2021. Construction will continue over a 12-month period. Rochester Avon Partners, LLC will invest an estimated \$10,125,000 in the development and create approximately 100 construction jobs and 70 permanent Full Time Equivalent (FTE) jobs in the first three years.

Preliminary site plans and renderings are included in Appendix C.

B. Basis of Eligibility (Section 13 (2)(h) and Section 2(o))

The Property is considered “Eligible Property” as defined by Act 381, Section 2 because the parcel located at 975 S. Rochester Road, which is a part of the subject property, has been determined to be a “Property” under Part 213 of P.A. 451, as amended and the rules promulgated thereunder. The unaddressed parcel (PID 70-15-14-351-068) and the parcel located a 945 S. Rochester Road are eligible parcels that are adjacent or contiguous and are estimated to increase the captured taxable value of the project.

Contaminant concentrations of volatile organic compounds (VOCs) that include benzene, toluene, ethylbenzene, xylenes, 1,2,4-trimethylbenze, naphthalene, and 2-methylnaphthalene were identified in the soil and groundwater at the subject property above Part 213 Residential and Nonresidential criteria for Drinking Water Protection (DWP)/Drinking Water (DW) and Groundwater Surface Water Interface Protection (GSIP)/Groundwater Surface Water Interface (GSI) Risk Based Screening Levels (RBSLs). Based on these analytical results and the closed Leaking Underground Storage Tank (LUST) status, the subject property is classified as a “Property” in accordance with Part 213 of P.A. 451 of 1994, as amended and the results promulgated thereunder.

Additional documentation and description of the property’s “Property” status is provided in Appendix D.

C. Summary of Eligible Activities and Description of Costs (Sec. 13 (2)(a-b))

Tax Increment Financing revenues will be used to reimburse the costs of “Eligible Activities” (as defined by Section 2 of Act 381) as permitted under the Brownfield Redevelopment Financing Act that include: Pre-Approved Activities, Department Specific Activities, Demolition, Asbestos Activities and preparation and implementation of a Brownfield Plan and Act 381 Work Plan. A complete itemization of these activities and associated expenses is included in Table 1.

The following eligible activities and budgeted costs are intended as part of the development of the property and are to be financed solely by the developer. All activities are intended to be “Eligible Activities” under the Brownfield Redevelopment Financing Act. The Authority is not responsible for any cost of eligible activities and will incur no debt.

1. Pre-Approved Activities include a Phase I and II Environmental Site Assessment (ESA), baseline environmental assessment (BEA), and Hazardous Materials Survey required as

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part of the pre-purchase due diligence conducted on the property at a total cost of \$15,325.

2. Department Specific Activities include Due Care Activities to facilitate redevelopment.

The developer is pursuing the Michigan Underground Storage Tank Authority (MUSTA) to address contamination on the property related to a confirmed release (REL-0223-19). MUSTA eligible costs are anticipated to include the removal and disposal of contaminated soil and groundwater above Part 213 Non-Residential Risk-Based Screening Level (RBSL) criteria, and the backfilling of the site. It is currently estimated that up to 1,500 tons of contaminated soil will be excavated and backfilled under MUSTA, however this volume may increase pending additional sampling, field conditions and approval from MUSTA. The costs covered by MUSTA will not be covered nor requested under the Brownfield Plan.

However, residual contamination is anticipated to remain in both soil and groundwater and additional Due Care Activities beyond the excavation funded by MUSTA will be required. These activities include the design and installation of a vapor barrier system; chemical-resistant gasketing for utilities; utility corridor migration barriers; underground storage tank (UST) removal; contaminated soil transport and disposal related to utility trenching, building footings etc., contaminated groundwater treatment and disposal, and oversight/sampling/reporting by an environmental professional at a total estimated cost of \$275,850.

Proper transport and disposal of contaminated soils remaining following MUSTA funded activities related to Due Care to facilitate development of the subject property are based on the following soil volume estimates, which are limited to the contaminated areas of the subject property:

Utility Trenching						
	Linear Feet	Estimate Pipe Diameter (")	Estimated Trench Depth (FT)	Estimated Trench Volume (CF) (Per linear foot, at 1:1 setback)	Estimated Trenching Volume for Disposal (CY)	Estimated Trenching Soil Weight for Disposal (TN)
Storm Sewer	200	18	5	20	148	244
TOTAL					148	244
Building Perimeter Footings						
Linear Feet	Estimated Trench Depth (FT)	Estimated Footing Width (")	Estimated Trench Volume (CF) (Per linear foot, at 1:1 setback)	Estimated Trenching Volume for Disposal (CY)	Estimated Trenching Soil Weight for Disposal (TN)	
100	4	24"	37.5	139	229	
TOTAL					139	229
Building Pad Cut (estimate assumes 12" cut)						
	Estimated Pad Area (SF)	Estimated Volume (CY)	Estimated Pad Cut Soil Weight for Disposal (TN) - Csoil Only			
Building	550	20	34			
TOTAL		20	34			

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Non-Building Pavement			
	Estimated Pavement Area (SF)	Estimated Volume (CY)	Estimated Pavement Cut Soil Weight for Disposal (TN)
Non-Building Pavement – (contaminated areas only)	12,000	369	609
TOTAL		369	609

3. Demolition Activities includes building demolition and the removal of parking lots at an estimated cost of \$120,000.
4. Asbestos Activities includes asbestos containing materials (ACM) abatement, oversight, air monitoring and associated reporting at an estimated cost of \$20,000.
5. Preparation and implementation of the Brownfield Plan and associated activities (e.g. meetings with BRA, review by City Attorney etc.) at a cost of approximately \$15,000.
6. A 15% contingency of \$62,378 is established to address unanticipated environmental and/or other conditions that may be discovered through the implementation of site activities. This excludes the cost of Pre-Approved Activities and preparation of the Brownfield Plan.

All activities are intended to be “Eligible Activities” under the Brownfield Redevelopment Financing Act. The total estimated cost of Eligible Activities subject to reimbursement from tax increment revenues is \$446,175 with a potential \$62,378 contingency, resulting in a total cost of \$508,553, the total cost for reimbursement to the applicant is a not-to-exceed amount of \$508,553 (including contingency), unless the Plan is amended and approved by the BRA and City Council.

D. Estimate of Captured Taxable Value and Tax Increment Revenues (Sec. 13 (2)(c))

Incremental taxes on real property included in the redevelopment project will be captured under this Plan to reimburse eligible activity expenses. The base taxable value of the subject property shall be determined by the use of the 2021 tax year tax values, which is \$617,020. Tax increment revenue capture will begin when tax increment is generated by redevelopment of the subject property, which is expected to begin in 2023 or when full redevelopment is completed whichever occurs first. The estimated post development taxable value of the completed development is \$2,216,954 and was provided by the City of Rochester Hills’ Assessor’s Office. Following completion of the project, the actual taxable value will be determined via the property assessment process by the City of Rochester Hills. An annual increase in taxable value of 1% has been used for calculation of future tax increments in this Plan. Tables 2 details the estimate of captured tax increment revenues for each year of the Plan from the eligible property.

The RHBRA has established a Local Brownfield Revolving Fund (LBRF). Per the RHBRA policy, capture for the LBRF is included in this plan for five (5) years following developer reimbursement, as well as an annual 3% capture of the total local increment revenue, currently estimated at \$183,318. The funds deposited into the LBRF as part of this Plan will be used in accordance with the requirements of Act 381, as amended.

Prior to reimbursement of tax increment revenue to the Developer, payment of Brownfield Redevelopment Authority Administrative fees, and the 3% capture for the Local Brownfield Revolving Fund (LBRF) will occur first. Additional LBRF capture will occur at the end of the Plan for five (5) years as shown in Table 2.

E. Method of Brownfield Plan Financing and Description of Advances by the Municipality (Sec. 13 (2)(d))

Eligible activities will be financed by Rochester Avon Partners, LLC. The Developer will be reimbursed for eligible costs as described in Section C and outlined in Table 1. Costs for Eligible Activities funded by Rochester Avon Partners, LLC will be repaid under the Michigan Brownfield Redevelopment Financing Program (Michigan Public Act 381, as amended) with incremental taxes generated by future development of the property. The estimated amount of tax increment revenue capture that will be used to reimburse the Developer, the Brownfield Redevelopment Authority Administration Fees, and LBRF is \$728,284.

No advances will be made by the RHBRA for this project. All reimbursements authorized under this Plan shall be governed by the Reimbursement Agreement.

F. Maximum Amount of Note or Bonded Indebtedness (Sec. 13 (2)(e))

No note or bonded indebtedness will be incurred by any local unit of government for this project.

G. Duration of Brownfield Plan (Sec. 13 (2)(f))

In no event shall the duration of the Plan, exceed 35 years following the date of the resolution approving the Plan, nor shall the duration of the tax capture exceed the lesser of the period authorized under subsection (4) and (5) of Section 13 of Act 381 or 30 years. The Property will become part of this Plan on the date this Plan is approved by the City of Rochester Hills City Council.

H. Estimated Impact of Tax Increment Financing on Revenues of Taxing Jurisdictions (Sec. 13 (2)(g))

Taxes will continue to be generated to taxing jurisdictions on local captured millages at the base taxable value of \$617,020 throughout the duration of this Plan totaling approximately \$248,100 or \$13,058 annually.

School millages; including the State Education Tax and the Rochester School Operating Millage, are not captured under this plan and will have an immediate increase in tax revenue following redevelopment of approximately \$825,922 throughout the duration of this Plan.

Non-capturable millages; including debt millages, the zoo authority and art institute, will have an immediate increase in tax revenue following redevelopment of approximately \$136,628 throughout the duration of this Plan.

A summary of the impact to taxing jurisdictions for the life of the Plan is summarized below, the amount captured is the sum of developer eligible activity reimbursement, administrative fees and the state brownfield revolving fund.

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Millage	Rate	Developer Reimbursement	Admin Fee	LBRF Capture	Taxes Preserved for Taxing Units
State Education	6.0000	\$0	\$0	\$0	\$276,821
School Operating	18.0000	\$0	\$0	\$0	\$830,462
Subtotal	24.0000	\$0	\$0	\$0	\$1,107,283
County Operating	4.0200	\$96,603	\$6,917	\$34,822	\$47,128
School Sinking	1.4874	\$35,743	\$2,559	\$12,884	\$17,437
Oak Int SD-Alloc	0.1918	\$4,609	\$330	\$1,661	\$2,249
Oak Int SD-Vtd.	3.0362	\$72,961	\$5,224	\$26,300	\$35,595
Oak Comm College	1.5184	\$36,488	\$2,613	\$13,153	\$17,801
Oakland County Parks	0.3500	\$8,411	\$602	\$3,032	\$4,103
Huron Clinton Parks	0.2104	\$5,056	\$362	\$1,823	\$2,467
General Fund	2.6909	\$64,664	\$4,630	\$23,309	\$31,546
Local Street	1.0868	\$26,116	\$1,870	\$9,414	\$12,741
Fire Fund	2.7000	\$64,882	\$4,646	\$23,388	\$31,653
Special Police I	1.1552	\$27,760	\$1,988	\$10,007	\$13,543
Special Police II	1.1891	\$28,575	\$2,046	\$10,300	\$13,940
Pathway	0.1773	\$4,261	\$305	\$1,536	\$2,079
RARA Operating	0.1861	\$4,472	\$320	\$1,612	\$2,182
OPC Operating	0.3200	\$7,690	\$551	\$2,772	\$3,751
OPC Transportation	0.0954	\$2,293	\$164	\$826	\$1,118
Library Operating	0.7478	\$17,970	\$1,287	\$6,478	\$8,767
Subtotal	21.1628	\$508,553	\$36,414	\$183,318	\$248,100
Total Capturable Millages	21.1628	\$508,553	\$36,414	\$183,318	\$248,100
Non-Capturable Millages by Brownfield TIF					
Rochester Sch. Debt	3.4800	\$0	\$0	\$0	\$160,556
Zoo Authority	0.0965	\$0	\$0	\$0	\$4,452
Art Institute	0.1913				\$8,826
Chapter 20 Drain Debt	0.0364	\$0	\$0	\$0	\$1,679
OPC Building Debt	0.1660	\$0	\$0	\$0	\$7,659
Total Non-Capturable Millages	3.9702	\$0	\$0	\$0	\$183,172

See Table 2 for a complete breakdown of estimated available tax increment revenue and Table 3 for the estimated annual reimbursement.

I. Legal Description, Property Map, Statement of Qualifying Characteristics and Personal Property (Sec. 13 (2)(h))

The legal description of the Property included in this Plan is attached in Appendix A.

Property location maps are included in Appendix B.

Documentation of characteristics that qualify the property as eligible property is provided in Appendix D.

Personal property may be included as part of the eligible property to the extent that it is taxable personal property.

J. Displacement/Relocation of Individuals on Eligible Property (Sec. 13 (2)(i-l))

No displacement of residents or families is expected as part of this project.

K. Other Material that the Authority or Governing Body Considers Pertinent (Sec. 13 (2)(n))

The Brownfield Redevelopment Authority and the City Council as the Governing Body, in accordance with the Act, may amend this Plan in order to fund additional eligible activities associated with the Project described herein.

Attachment A



Legal Descriptions

975 South Rochester Road, Rochester Hills, Oakland County, Michigan:

Parcel: 70-15-14-351-012

T3N, R11E, SEC 14 PART OF SW 1/4 BEG AT PT DIST S 89-51-30 E 33 FT & N 60 FT FROM SW SEC COR, TH N 135 FT, TH E 162.50 FT, TH S 135 FT, TH W 162.50 FT TO BEG 0.51 AB203A

945 South Rochester Road, Rochester Hills, Oakland County, Michigan:

Parcel: 70-15-14-351-011

T3N, R11E, SEC 14 PART OF SW 1/4 BEG AT PT DIST S 89-51-30 E 33 FT & N 195 FT FROM SW 1/4 COR, TH N 135 FT, TH E 162.50 FT, TH S 135 FT, TH W 162.50 FT TO BEG 0.50 AB203B

Unaddressed Parcel, Rochester Hills, Oakland County, Michigan:

Parcel: 70-15-14-351-068

T3N, R11E, SEC 14 PART OF SW 1/4 BEG AT PT DIST S 89-51-30 E 195.50 FT & N 60 FT FROM SW SEC COR, TH N 270 FT, TH S 89-51-30 E 58 FT, TH S 24.30 FT, TH S 89-51-30 E 85 FT, TH N 24.30 FT, TH S 89-51-30 E 57 FT, TH S 270 FT, TH N 89-51-30 W 200 FT TO BEG 1.19 A 11-12-20 FR 062

Attachment B











Map



Unaddressed
(Parcel: 70-15-14-351-068)
"Adjacent and Contiguous"


945 S. Rochester Road
(Parcel: 70-15-14-351-011)
"Adjacent and Contiguous"

975 S. Rochester Road
(Parcel: 70-15-14-351-012)
"Property"

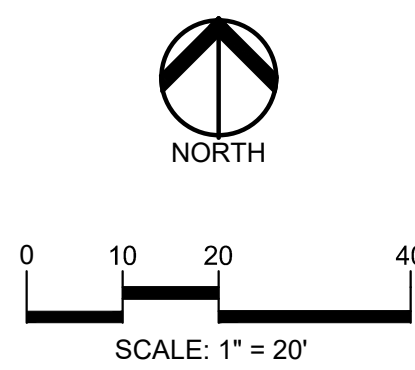
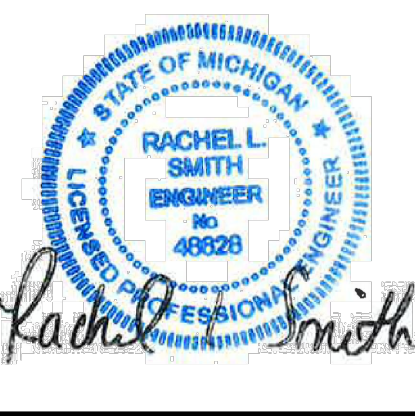
-  2 Foot Contours
-  5 Foot Contours
-  FEMA Base Flood Elevations
-  FEMA Cross Sections
-  100 yr - FEMA Floodplain
-  100 yr (detailed) - FEMA Floodplain
-  500 yr - FEMA Floodplain
-  FLOODWAY - FEMA Floodplain

Disclaimer: The information provided herewith has been compiled from recorded deeds, plats, tax maps, surveys and other public records. It is not a legally recorded map or survey and is not intended to be used as one. Users should consult the information sources mentioned above when questions arise. FEMA Floodplain data may not always be present on the map.

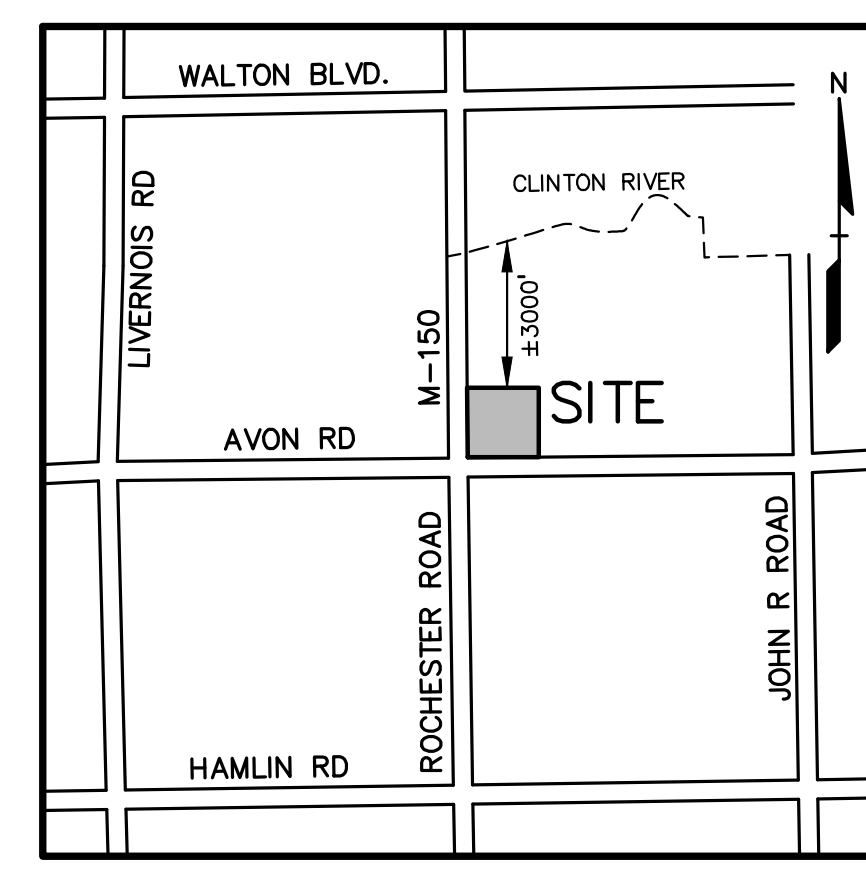

L. Brooks Patterson
 Oakland County Executive

Date Created: 8/14/2019

 NORTH
 1 inch = 200 feet

W 1/4 CORNER SECTION 14
T.03N., R.11E
CITY OF ROCHESTER HILLS
OAKLAND COUNTY, MI
REMON IN MON BOX



CAUTION!!
THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL EXERCISE RESPONSIBILITY FOR DETERMINING THE EXACT UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.



LEGEND

● IRON FOUND	● BRASS PLUG SET	● SEC. CORNER FOUND
⊗ IRON SET	⊗ MONUMENT SET	○ RECORDED
⊗ NAIL FOUND	⊗ MONUMENT SET	M MEASURED
⊗ NAIL & CAP SET		C CALCULATED

EXISTING

- OH-ELEC-W-O ELEC. PHONE OR CABLE TV OH LINE, POLE & GUY WIRE
- UG-CATV-UG UNDERGROUND CABLE TV, CATV PEDESTAL
- UG-PW-C-UG TELEPHONE U.G. CABLE, FIBERGLASS MANHOLE
- UG-ELEC-ELEC-ELECTRIC U.G. CABLE, MANHOLE, METER & HANDHOLE
- UG-GAS-GAS GAS MAIN, VALVE & GAS LINE MARKER
- WATERMAN-HYD. GATE VALVE, TAPPING SLEEVE & VALVE
- SANITARY SEWER, CLEANOUT & MANHOLE
- STORM SEWER, CLEANOUT & MANHOLE
- SQ. ROUND & RECT. CATCH BASIN, YARD DRAIN
- WATER VALVE, BACKFLOW PREVENTER, VALVE BOX, SERVICE SHUTOFF
- WALKBOX, TRANSFORMER, SWITCHING CONTROL VALVE
- UNDERTRENCH STRUCTURE
- SPOT ELEVATION
- CONTOUR LINE
- FENCE
- GUARD RAIL
- STREET LIGHT
- SKIN
- CONC. CONCRETE
- ASPH. ASPHALT

REFERENCE DRAWINGS

- WATER MAIN CITY OF ROCHESTER HILLS GIS MAP
- SANITARY SEWER CITY OF ROCHESTER HILLS GIS MAP
- STORM SEWER CITY OF ROCHESTER HILLS GIS MAP
- ELECTRIC NOT RECEIVED
- TELEPHONE NOT RECEIVED
- GAS CONSUMERS ENERGY MAP# 03-61-14-3, DATED 01/10/19
- NO FACILITIES IN THE AREA
- PETROLEUM COMMERCE SERVICE MAP RECEIVED 02/19/19
- FLOOD PLAN FIRM MAP 26125C0392F, DATED 08/29/06, ZONE "X"
- MDOT ROAD PLANS, VARIOUS PROJECTS
- BOUNDARY TOPOGRAPHIC SURVEY, COMERICA BANK, #FMM48, DATED 12/21/10
- OTHER PARTIAL GRADING/UTILITY PLAN, COMERICA BANK, UNDATED

CLIENT
ROCHESTER AVON PARTNERS, LLC
251 EAST MERRILL STREET, SUITE #205
BIRMINGHAM, MICHIGAN, 48009

PROJECT TITLE
RETAIL REDEVELOPMENT
N.E.C. ROCHESTER ROAD & AVON ROAD
CITY OF ROCHESTER HILLS, OAKLAND COUNTY, MI

REVISIONS

ORIGINAL ISSUE DATE:
MAY 10, 2021

DRAWING TITLE
TOPOGRAPHIC SURVEY

PEA JOB NO.	2019-007
P.M.	RLS
DN.	PB
DES.	PB
DRAWING NUMBER:	C-1.0

BENCHMARKS
(GPS DERIVED - NAVD88)

BM #300
CUT "X" ON NORTHEAST BOLT HYDRANT EAST SIDE ROCHESTER ROAD, ±350' NORTH OF AVON ROAD.
ELEV. - 846.92'

BM #301
CUT "X" ON SOUTHWEST BOLT OF STEEL STRAIN POLE AT NORTHEAST CORNER OF AVON AND ROCHESTER ROAD.
ELEV. - 844.05'

BM #302
SET BENCHMARK IN NORTH FACE OF UTILITY POLE NORTH SIDE OF AVON ROAD, ±300' EAST OF ROCHESTER ROAD.
ELEV. - 838.42'

EXISTING SOIL INFORMATION:

PER THE WEB SOIL SURVEY OF OAKLAND COUNTY, AS PUBLISHED BY THE USDA-NRCS, VERSION 19, DATED JUNE 1, 2020, THIS SITE CONSISTS OF SOILS OF THE FOLLOWING TYPE:

59 - URBAN LAND

LEGAL DESCRIPTION
COMBINED PROPERTY

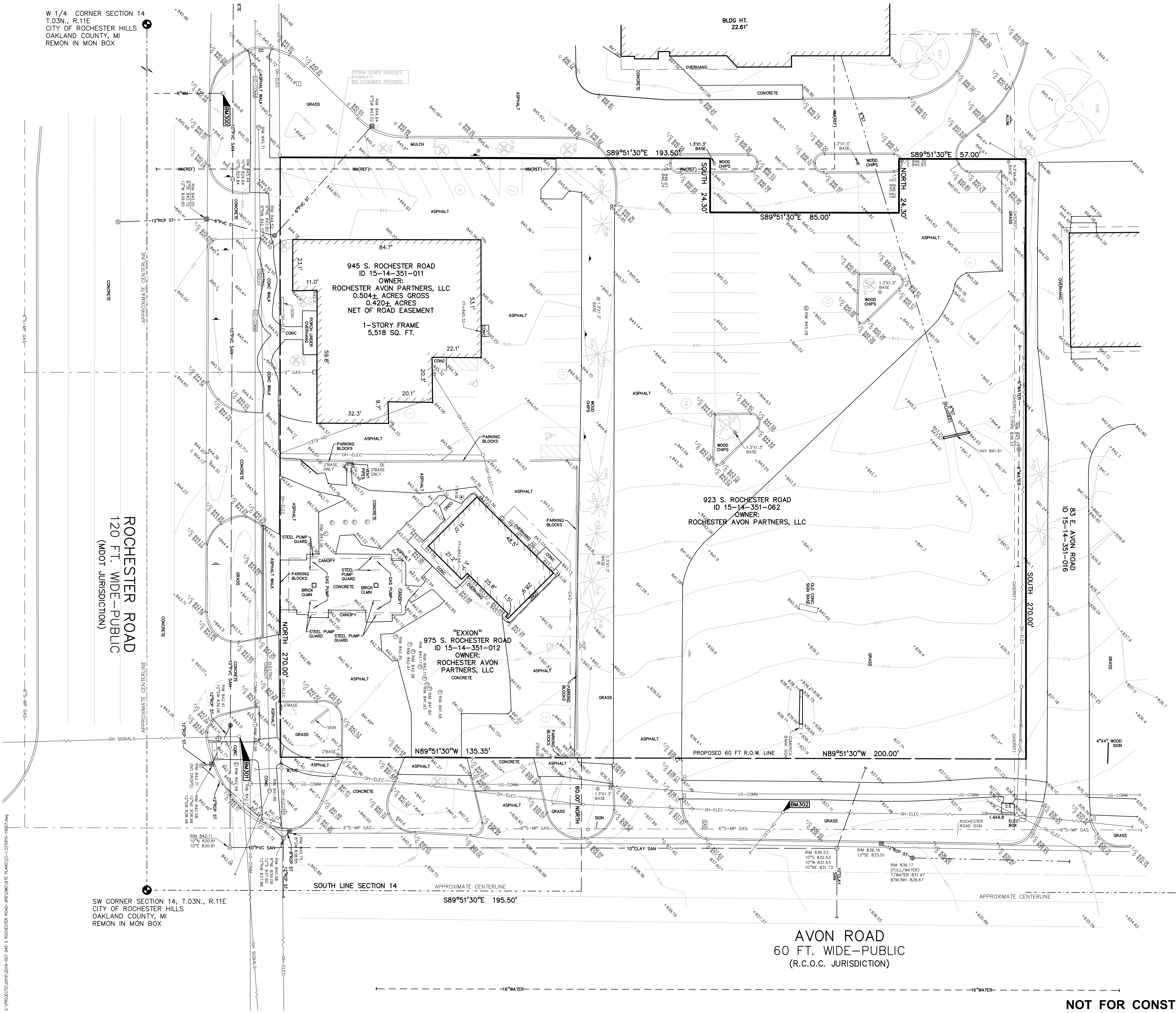
LAND IN A PART OF SOUTHWEST 1/4 SECTION 14, TOWN 3 NORTH, RANGE 11 EAST, CITY OF ROCHESTER HILLS, OAKLAND COUNTY, MICHIGAN, DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST SECTION CORNER OF SAID SECTION 14; THENCE SOUTH 89 DEGREES 51 MINUTES 30 SECONDS EAST 195.50 FEET ALONG THE SOUTH LINE OF SECTION 14; THENCE NORTH 60.00 FEET TO THE NORTH LINE OF AVON ROAD (120 FEET WIDE) AND THE POINT OF BEGINNING; THENCE ALONG SAID NORTH LINE NORTH 89 DEGREES 51 MINUTES 30 SECONDS WEST 135.35 FEET TO THE EAST LINE OF S. ROCHESTER ROAD (WIDTH VARIES); THENCE ALONG SAID EAST LINE NORTH 27.00 FEET; THENCE SOUTH 89 DEGREES 51 MINUTES 30 SECONDS EAST 193.50 FEET; THENCE SOUTH 24.30 FEET; THENCE SOUTH 89 DEGREES 51 MINUTES 30 SECONDS EAST 85.00 FEET; THENCE NORTH 24.30 FEET; THENCE S89°51'30"E 57.00 FEET; THENCE SOUTH 27.00 FEET TO THE NORTH LINE OF SAID AVON ROAD; THENCE ALONG SAID NORTH LINE NORTH 89 DEGREES 51 MINUTES 30 SECONDS WEST 200.00 FEET TO THE POINT OF BEGINNING, CONTAINING ±2.03 ACRES

FLOODPLAIN:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN "AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN" (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP PANEL NUMBER 26125C0392F, EFFECTIVE DATE SEPTEMBER 29, 2006

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NOT FOR CONSTRUCTION



SW CORNER SECTION 14, T.03N., R.11E
CITY OF ROCHESTER HILLS
OAKLAND COUNTY, MI
REMON IN MON BOX

AVON ROAD
60 FT. WIDE-PUBLIC
(R.C.O.C. JURISDICTION)

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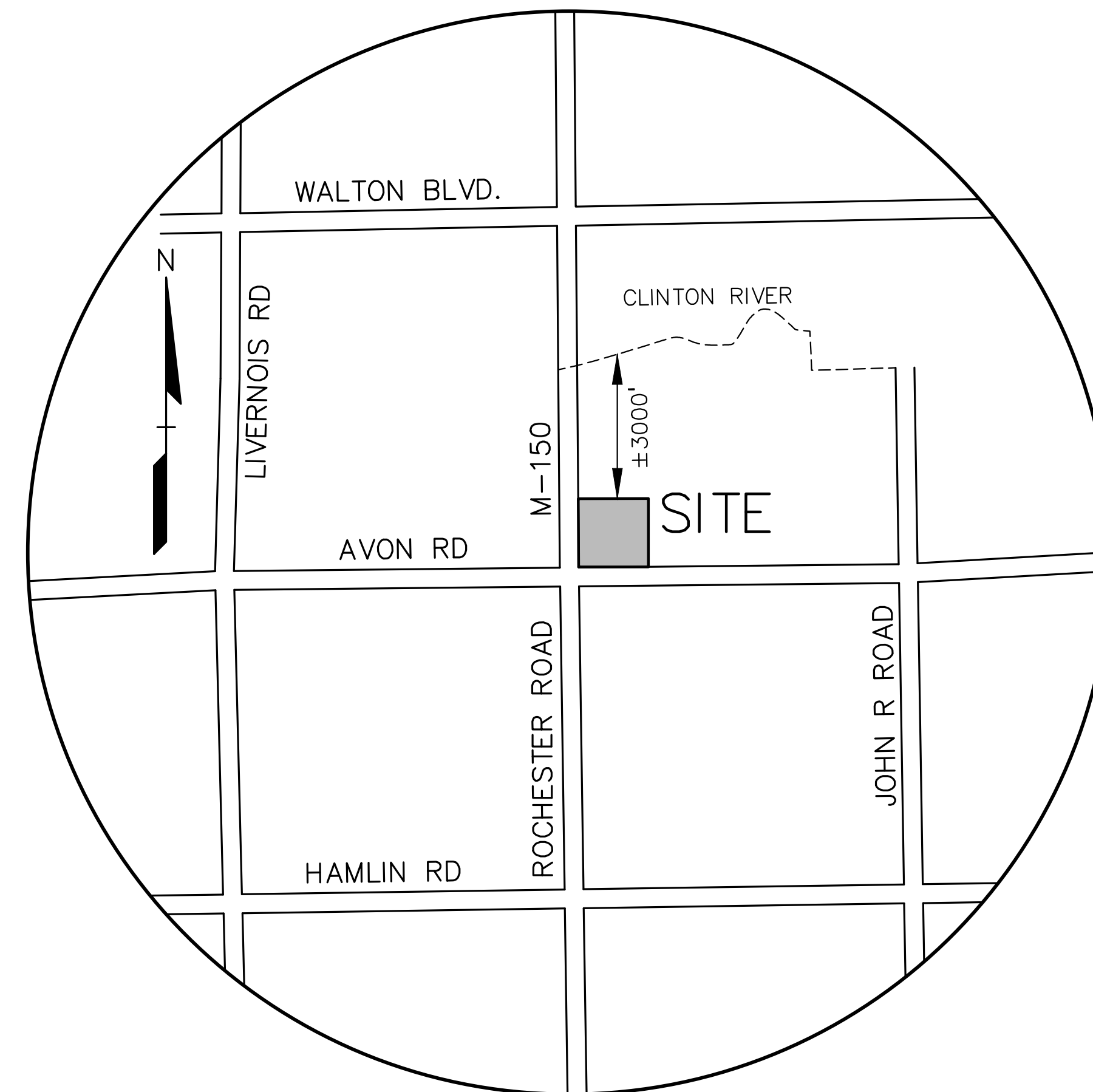
Attachment C



PRELIMINARY SITE PLANS FOR
ROCHESTER RETAIL
 N.E.C. OF ROCHESTER ROAD AND AVON ROAD
 CITY OF ROCHESTER HILLS, OAKLAND COUNTY, MICHIGAN



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LOCATION MAP
 NO SCALE

INDEX OF DRAWINGS:

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PHOTOMETRIC PLANS:

SHEET 1	OF 2 LED SITE LAYOUT
SHEET 2	OF 2 LED SITE LAYOUT

CLIENT
ROCHESTER AVON PARTNERS, LLC
 251 EAST MERRILL STREET, SUITE #205
 BIRMINGHAM, MICHIGAN, 48009

PROJECT TITLE
RETAIL REDEVELOPMENT
 N.E.C. ROCHESTER ROAD & AVON ROAD
 CITY OF ROCHESTER HILLS, OAKLAND COUNTY, MI

REVISIONS	

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COVER SHEET

PEA JOB NO. 2019-007

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COVER

ROCHESTER HILLS FIRE DEPARTMENT NOTES:

- A KNOX KEY SYSTEM SHALL BE INSTALLED, IN A LOCATION APPROVED BY THE FIRE CODE OFFICIAL. ORDERING INFORMATION IS AVAILABLE FROM THE KNOX COMPANY AT www.knoxbox.com (3200 SERIES BOX).
- FIRE LANES SHALL BE DESIGNATED BY THE FIRE CODE OFFICIAL AND SHALL BE CONSPICUOUSLY POSTED ON BOTH SIDES OF THE FIRE LANE, WITH FIRE LANE SIGNS SPACED NOT MORE THAN 100 FEET APART. FIRE LANE SIGNS SHALL READ "NO STOPPING, STANDING, PARKING, FIRE LANE", AND SHALL CONFORM TO THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- CONSTRUCTION SITES SHALL BE SAFEGUARDED IN ACCORDANCE WITH THE INTERNATIONAL FIRE CODE (IFC) 2006 CHAPTER 14.
- OPEN BURNING IS NOT PERMITTED, INCLUDING THE BURNING OF TRASH, DEBRIS OR LAND CLEARING MATERIALS. OPEN BURNING FOR WARMING OF SAND AND/OR WATER FOR THE PREPARATION OF MORTAR SHALL BE WITHIN THE CITY OF ROCHESTER HILLS BURN PERMIT GUIDELINES.
- PROVIDE A "NO PARKING FIRE DEPARTMENT CONNECTION" SIGN OVER THE FIRE DEPARTMENT CONNECTION.

DEVELOPER:

ROCHESTER AVON PARTNERS, LLC
 251 EAST MERRILL STREET, SUITE #205
 BIRMINGHAM, MI, 48009
 CONTACT: DORAID MARKUS
 PHONE: (248) 892-2222
 EMAIL: DMARKUS@MARKUSLLC.COM

ARCHITECT:

ROGVOY ARCHITECTS
 32500 TELEGRAPH ROAD, SUITE #250
 BINGHAM FARMS, MI, 48025
 CONTACT: MARK DRANE
 PHONE: (248) 540-7700
 FAX: (248) 540-2710

CIVIL ENGINEER:

PEA GROUP
 2430 ROCHESTER COURT, SUITE 100
 TROY, MI, 48063
 CONTACT: RACHEL L. SMITH, PE
 PHONE: (248) 689-9090 EXT. 1165
 FAX: (248) 689-1044
 EMAIL: RACHEL.SMITH@PEAGROUP.COM

LANDSCAPE ARCHITECT:

PEA GROUP
 7927 NEMCO WAY, SUITE 115
 BRIGHTON, MI 48116
 CONTACT: JEFF SMITH, R.L.A., LEED AP
 PHONE: (517) 546-8583
 FAX: (517) 546-8973
 EMAIL: JSMITH@PEAGROUP.COM

NOT FOR CONSTRUCTION

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 XREF: S:\PROJECTS\2019\2019-007\DWG\SITE PLANS\BLK-19007.dwg

W 1/4 CORNER SECTION 14
T.03N., R.11E
CITY OF ROCHESTER HILLS
OAKLAND COUNTY, MI
REMON IN MON BOX

RAMP LANDINGS TO BE
CONCRETE WITH DETECTABLE
WARNING AND ARE TO BE
INSTALLED AT MAX. SLOPE OF 2%

8' WIDE ASPHALT PATH, TYP.
REFER TO DETAILS ON C-9.1

HEAVY DUTY ASPHALT
PAVEMENT, TYP. REFER TO
DETAIL ON SHEET C-9.1

STANDARD DUTY ASPHALT
PAVEMENT, TYP. REFER TO
DETAIL ON SHEET C-9.1

SEGMENTAL BLOCK RETAINING
WALL ALONG PROPERTY LINE.
HEIGHT VARIES (0"-12")

AMENITY AREA (SHADED)
PATH, BENCHES, BIKE RACKS,
ETC. AREA = 1,795 SQ.FT.
REFER TO LANDSCAPE PLANS
FOR ADDITIONAL DETAILS

1.5' VEHICLE OVERHANG, TYP.

18"x6" CONCRETE CURB
AND GUTTER, TYP. REFER
TO DETAIL ON SHEET C-9.1

CONCRETE CURB
SPILLWAY, REFER TO
DETAIL ON SHEET C-9.1

PARKING SPACES FOR
COMERICA BANK

STANDARD DUTY ASPHALT
PAVEMENT, TYP. REFER TO
DETAIL ON SHEET C-9.1

DETAIL M GUTTER PAN, TYP.
REFER TO M.D.O.T. DETAIL R-29
ON CONSTRUCTION PLANS

30" WIDE F-5 CURB AND GUTTER.
REFER TO M.D.O.T. DETAIL R-30
ON CONSTRUCTION PLANS

RIGHT-IN, LEFT-IN AND RIGHT-OUT
DRIVE APPROACH. REFER TO
M.D.O.T. DETAIL GEO-680

NOTE:
ALL WORK WITHIN THE ROCHESTER
ROAD (M-150) RIGHT-OF-WAY IS
UNDER THE JURISDICTION OF THE
MICHIGAN DEPT. OF TRANSPORTATION
AND REQUIRES A PERMIT.

ALL SIGNS MUST MEET CHAPTER 134 OF
THE CITY CODE OF ORDINANCES AND BE
APPROVED UNDER A SEPARATE PERMIT
ISSUED BY THE BUILDING DEPARTMENT

CONCRETE SIDEWALK, TYP.
REFER TO DETAILS ON C-9.1

8' WIDE ASPHALT PATH, TYP.
REFER TO DETAILS ON C-9.1

CONCRETE SIDEWALK, TYP.
REFER TO DETAILS ON C-9.1

8' WIDE ASPHALT PATH, TYP.
REFER TO DETAILS ON C-9.1

18"x6" CONCRETE CURB
AND GUTTER, TYP. REFER
TO DETAIL ON SHEET C-9.1

30" WIDE F-5 CURB AND GUTTER.
REFER TO M.D.O.T. DETAIL R-30
ON CONSTRUCTION PLANS

EXISTING CONCRETE SIDEWALK,
SIDEWALK RAMPS AND DETECTABLE
WARNING TO REMAIN AT ROAD
INTERSECTION, AS SHOWN.

SW CORNER SECTION 14, T.03N., R.11E
CITY OF ROCHESTER HILLS
OAKLAND COUNTY, MI
REMON IN MON BOX

24"x6" CONCRETE CURB
AND GUTTER, TYP. REFER
TO DETAIL ON SHEET C-9.1

CONCRETE SIDEWALK, TYP.
REFER TO DETAILS ON C-9.1

8' WIDE ASPHALT PATH, TYP.
REFER TO DETAILS ON C-9.1

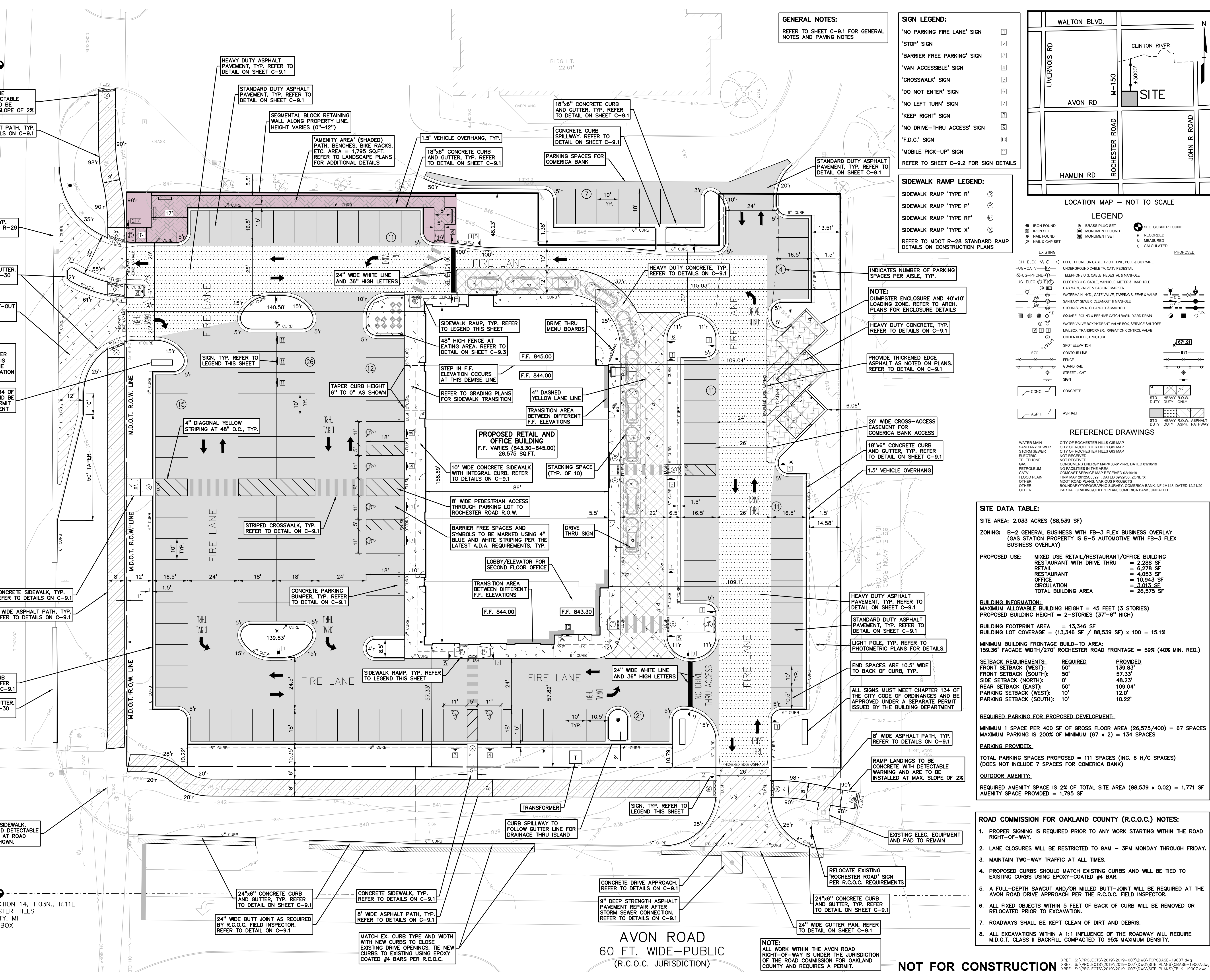
CONCRETE DRIVE APPROACH,
REFER TO DETAILS ON C-9.1

9" DEEP STRENGTH ASPHALT
PAVEMENT REPAIR AFTER
STORM SEWER CONNECTION
REFER TO DETAILS ON C-9.1

24"x6" CONCRETE CURB
AND GUTTER, TYP. REFER
TO DETAIL ON SHEET C-9.1

24" WIDE BUTT JOINT AS REQUIRED
BY R.C.O.C. FIELD INSPECTOR.
REFER TO DETAIL ON C-9.1

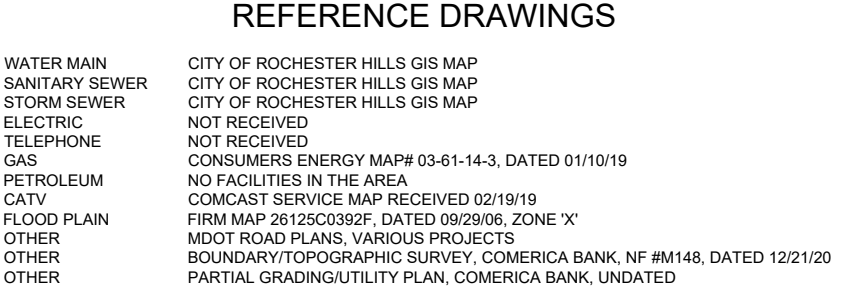
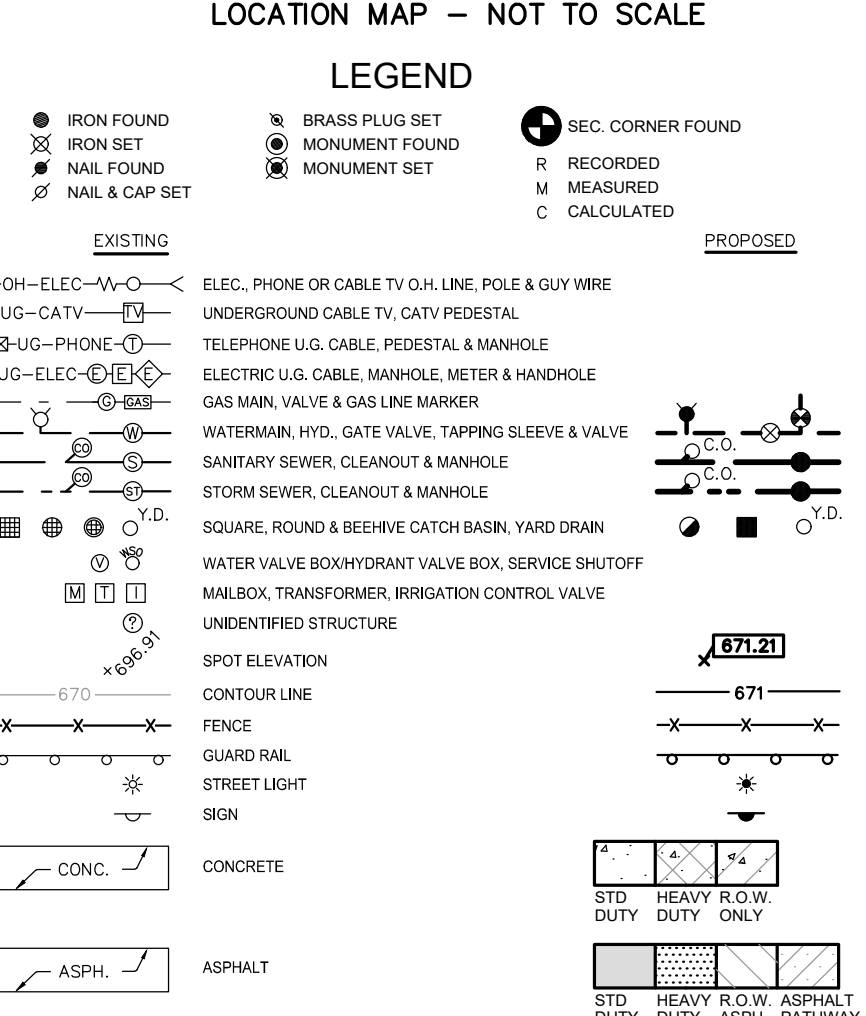
MATCH EX. CURB TYPE AND WIDTH
WITH NEW CURBS TO CLOSE
EXISTING DRIVE OPENINGS. THE NEW
CURBS TO EXISTING USING EPOXY
COATED #4 BARS PER R.C.O.C.



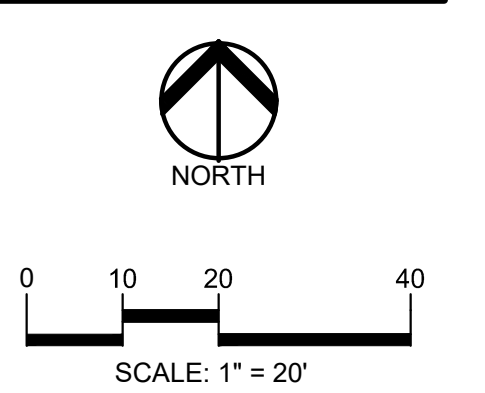
GENERAL NOTES:
REFER TO SHEET C-9.1 FOR GENERAL
NOTES AND PAVING NOTES

SIGN LEGEND:
 'NO PARKING FIRE LANE' SIGN 1
 'STOP' SIGN 2
 'BARRIER FREE PARKING' SIGN 3
 'VAN ACCESSIBLE' SIGN 4
 'CROSSWALK' SIGN 5
 'DO NOT ENTER' SIGN 6
 'NO LEFT TURN' SIGN 7
 'KEEP RIGHT' SIGN 8
 'NO DRIVE-THRU ACCESS' SIGN 9
 'F.D.C.' SIGN 10
 'MOBILE PICK-UP' SIGN 11
 REFER TO SHEET C-9.2 FOR SIGN DETAILS

SIDEWALK RAMP LEGEND:
 SIDEWALK RAMP 'TYPE R'
 SIDEWALK RAMP 'TYPE P'
 SIDEWALK RAMP 'TYPE RF'
 SIDEWALK RAMP 'TYPE X'
 REFER TO MDOT R-28 STANDARD RAMP
DETAILS ON CONSTRUCTION PLANS



PEA GROUP
t: 844.813.2949
www.peagroup.com



CAUTION!!
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UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY
APPROXIMATE. NO GUARANTEE AS TO ACCURACY THEREOF.
THE CONTRACTOR SHALL EXERCISE RESPONSIBILITY FOR
DETERMINING THE EXACT UTILITY LOCATIONS AND ELEVATIONS
PRIOR TO THE START OF CONSTRUCTION.

SITE DATA TABLE:
SITE AREA: 2.033 ACRES (88,539 SF)

ZONING: B-2 GENERAL BUSINESS WITH FB-3 FLEX BUSINESS OVERLAY
(GAS STATION PROPERTY IS B-5 AUTOMOTIVE WITH FB-3 FLEX
BUSINESS OVERLAY)

PROPOSED USE: MIXED USE RETAIL/RESTAURANT/OFFICE BUILDING
 RESTAURANT WITH DRIVE THRU = 2,288 SF
 RETAIL = 4,278 SF
 RESTAURANT = 4,053 SF
 OFFICE = 10,943 SF
 CIRCULATION = 3,013 SF
 TOTAL BUILDING AREA = 26,575 SF

BUILDING INFORMATION:
 MAXIMUM ALLOWABLE BUILDING HEIGHT = 45 FEET (3 STORIES)
 PROPOSED BUILDING HEIGHT = 2-STORIES (37'-6" HIGH)
 BUILDING FOOTPRINT AREA = 13,346 SF
 BUILDING LOT COVERAGE = (13,346 SF / 88,539 SF) x 100 = 15.1%

MINIMUM BUILDING FRONTAGE BUILD-TO-AREA:
 159.36' FAÇADE WIDTH/270' ROCHESTER ROAD FRONTAGE = 58% (40% MIN. REQ.)

SETBACK REQUIREMENTS:
 FRONT SETBACK (WEST): 50' 139.83'
 FRONT SETBACK (SOUTH): 50' 57.33'
 SIDE SETBACK (NORTH): 0' 48.23'
 REAR SETBACK (EAST): 50' 109.04'
 PARKING SETBACK (WEST): 10' 12.0'
 PARKING SETBACK (SOUTH): 10' 10.22'

REQUIRED PARKING FOR PROPOSED DEVELOPMENT:
 MINIMUM 1 SPACE PER 400 SF OF GROSS FLOOR AREA (26,575/400) = 67 SPACES
 MAXIMUM PARKING IS 200% OF MINIMUM (67 x 2) = 134 SPACES

PARKING PROVIDED:
 TOTAL PARKING SPACES PROPOSED = 111 SPACES (INC. 6 H/C SPACES)
 (DOES NOT INCLUDE 7 SPACES FOR COMERICA BANK)

OUTDOOR AMENITY:
 REQUIRED AMENITY SPACE IS 2% OF TOTAL SITE AREA (88,539 x 0.02) = 1,771 SF
 AMENITY SPACE PROVIDED = 1,795 SF

ROAD COMMISSION FOR OAKLAND COUNTY (R.C.O.C.) NOTES:

1. PROPER SIGNING IS REQUIRED PRIOR TO ANY WORK STARTING WITHIN THE ROAD RIGHT-OF-WAY.
2. LANE CLOSURES WILL BE RESTRICTED TO 9AM - 3PM MONDAY THROUGH FRIDAY.
3. MAINTAIN TWO-WAY TRAFFIC AT ALL TIMES.
4. PROPOSED CURBS SHOULD MATCH EXISTING CURBS AND WILL BE TIED TO EXISTING CURBS USING EPOXY-COATED #4 BAR.
5. A FULL-DEPTH SAWCUT AND/OR MILLED BUTT-JOINT WILL BE REQUIRED AT THE AVON ROAD DRIVE APPROACH PER THE R.C.O.C. FIELD INSPECTOR.
6. ALL FIXED OBJECTS WITHIN 5 FEET OF BACK OF CURB WILL BE REMOVED OR RELOCATED PRIOR TO EXCAVATION.
7. ROADWAYS SHALL BE KEPT CLEAN OF DIRT AND DEBRIS.
8. ALL EXCAVATIONS WITHIN A 1:1 INFLUENCE OF THE ROADWAY WILL REQUIRE M.D.O.T. CLASS II BACKFILL COMPACTED TO 95% MAXIMUM DENSITY.

CLIENT
ROCHESTER AVON PARTNERS, LLC
 251 EAST MERRILL STREET, SUITE #205
 BIRMINGHAM, MICHIGAN, 48009

PROJECT TITLE
RETAIL REDEVELOPMENT
 N.E.C. ROCHESTER ROAD & AVON ROAD
 CITY OF ROCHESTER HILLS, OAKLAND COUNTY, MI

REVISIONS

NO.	DATE	DESCRIPTION

ORIGINAL ISSUE DATE:
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DRAWING TITLE
SITE PLAN

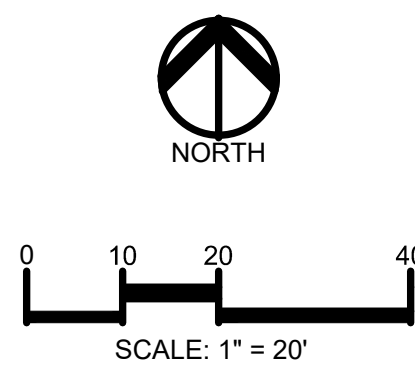
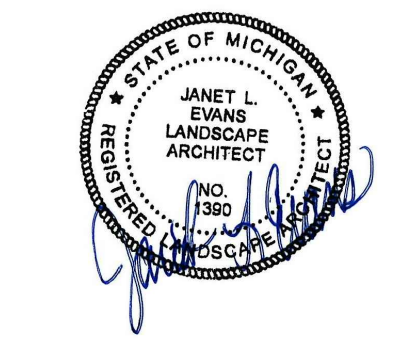
PEA JOB NO.	2019-007
P.M.	RLS
DN.	PB
DES.	PB
DRAWING NUMBER:	C-3.0

AVON ROAD
 60 FT. WIDE-PUBLIC
 (R.C.O.C. JURISDICTION)

NOTE:
 ALL WORK WITHIN THE AVON ROAD
 RIGHT-OF-WAY IS UNDER THE JURISDICTION
 OF THE ROAD COMMISSION FOR OAKLAND
 COUNTY AND REQUIRES A PERMIT.

NOT FOR CONSTRUCTION

REVISIONS:
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251 EAST MERRILL STREET, SUITE #205
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N.E.C. ROCHESTER ROAD & AVON ROAD
CITY OF ROCHESTER HILLS, OAKLAND COUNTY, MI

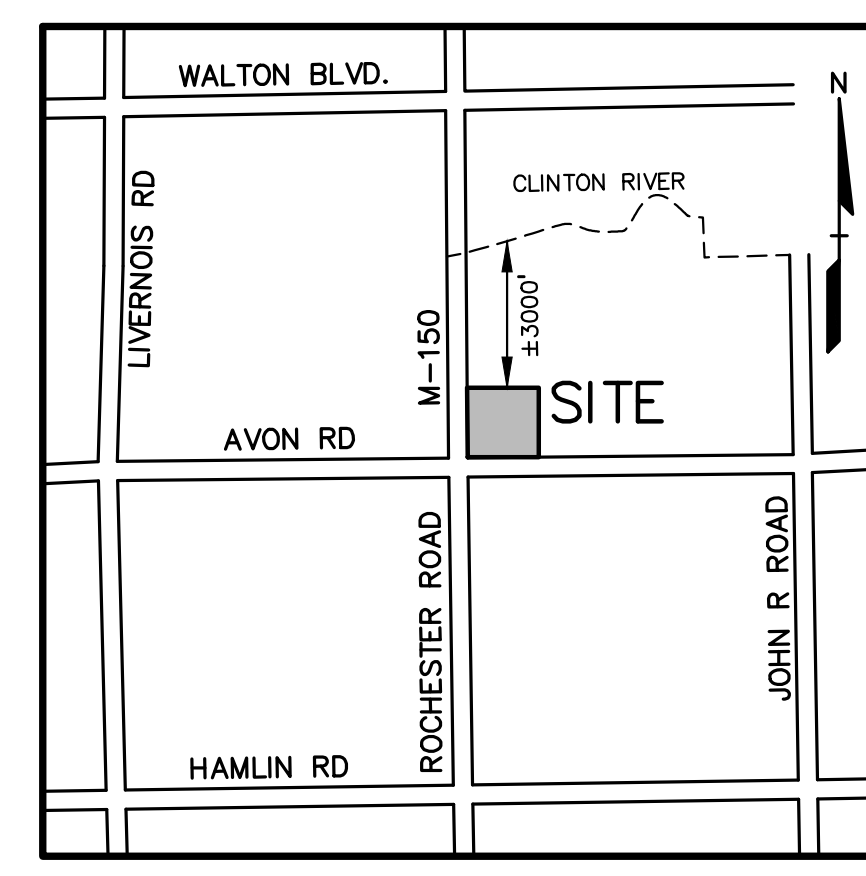
NO.	REVISIONS

ORIGINAL ISSUE DATE:
MAY 10, 2021

DRAWING TITLE
LANDSCAPE PLAN

PEA JOB NO.	2019-007
P.M.	RLS
DN.	JE
DES.	JE
DRAWING NUMBER:	

L-1.0



LANDSCAPE CALCULATIONS:
PER CITY OF ROCHESTER HILLS ZONING ORDINANCE, ZONED B-2 WITH FB-3 FLEX BUSINESS OVERLAY DISTRICT

R.O.W. TREES:
REQUIRED: 1 DEC OR EVG/ 35LF + 1 ORN TREE/ 60LF OF ROW.
AVON RD: 335'/35 = 10 DEC OR EVG AND 335'/60 = 6 ORN.
ROCHESTER RD: 290'/35 = 9 DEC OR EVG AND 290'/60 = 5 ORN
PROVIDED:
AVON RD: 10 DEC OR EVG, 6 ORN
ROCHESTER RD: 9 DEC OR EVG, 5 ORN TREES

FRONT YARD LANDSCAPING:
REQUIRED: 3 DEC, 4 ORN, 15 SHRUBS/ 100LF OF ROW. (15' WIDE)
ROCHESTER RD: 290'/100 = 9 DEC, 12 ORN. AND 44 SHRUBS
PROVIDED: 9 TREES AT DRIVE THRU, 4 ORN. AT ROCHESTER RD, 4 ORN. AT AVON RD., 4 ORN. AT EAST P/L AND 44 SHRUBS AT ROCHESTER RD. (NOT ENOUGH ROOM TO BE PLACED IN ROCHESTER RD. ROW, TREES ARE LOCATED ELSEWHERE ON SITE)

INTERIOR PARKING LANDSCAPE:
REQUIRED: MINIMUM 5% OF THE VEHICLE USE AREA TO BE LANDSCAPED AREA. 1 DEC. TREE FOR EVERY 150 SF OF REQUIRED INTERIOR LANDSCAPE.
5% OF THE 47,274 SF VEHICLE USE AREA = 2364 /150 = 16 TREES
PROVIDED: 16 TREES AND HEDGE AT PARKING SPACES

OUTDOOR AMENITY SPACE:
REQUIRED: MINIMUM 2% GROSS LAND AREA OF THE DEVELOPMENT TO BE OUTDOOR AMENITY SPACE.
GROSS LAND AREA: 88,539 SF * .02 = 1,771 SF REQUIRED
PROVIDED: 1,795 SF OF AMENITY SPACE. WITH PLANTINGS AND BENCHES/ BIKE RACK

TREE REPLACEMENT CALCULATIONS:
PER CITY OF ROCHESTER HILLS ZONING ORDINANCE
REPLACEMENT TREES:
REQUIRED: 1, 2" DEC. TREE OR 8' EVG PER REGULATED TREE REMOVED. 2" TREE REPLACEMENT CREDIT FOR EACH SPECIMEN TREE PRESERVED.
REPLACEMENT TREES:
REQUIRED: 18, 3" OR 8' EVG. REPLACEMENT TREES (8 REMAINING TREES TO BE PUT INTO TREE FUND DUE TO SPACE LIMITATIONS, 304.00 X 8 = \$2,432.00)
SEE SHEET T-1.0 FOR TREE PRESERVATION PLAN

KEY:

- = R.O.W. FRONTAGE TREES
- = ARTERIAL FRONT YARD TREES (PLACED ELSEWHERE ON SITE)
- = INTERIOR PARKING LOT TREES
- = REPLACEMENT TREE
- = IRRIGATED SHRUBS
- = IRRIGATED AMENITY AREA PLANTINGS TO BE DETAILED DURING CONST. DWGS
- = IRRIGATED SOD LAWN
- = ROCK MAINTENANCE EDGE, 6" DEPTH RIVER ROCK OVER WEED FABRIC
- = TREE PROTECTION FENCING

IRRIGATION NOTES:
ALL LANDSCAPE AREAS MUST BE IRRIGATED. WATERING WILL ONLY OCCUR BETWEEN THE HOURS OF 12AM-5AM. AN IRRIGATION PLAN MUST BE SUBMITTED PRIOR TO CITY APPROVAL OF THE FINAL SITE PLAN.

CITY OF ROCHESTER HILLS NOTES:

TREE PLANTING RESTRICTIONS:
Prior approval is required to plant any tree or shrub on the public right-of-way. All trees and shrubs must be planted at least 10' from the edge of the public road. Shade trees and shrubs must be planted at least 5' from the edge of the public roadway. Evergreen and ornamental trees must be planted at least 10' from the edge of the public roadway. No trees or shrubs may be planted within the triangular area formed at the intersection of any street right-of-way lines at a distance along each line of 25' from their point of intersection. No trees or shrubs may be planted in the triangular area formed at the intersection of any driveway with a public roadway at a distance along each line of 15' from their point of intersection. All trees and shrubs must be planted at least 10' from any fire hydrant. Shade and evergreen trees must be at least 15' away from the nearest overhead wire. Trees must be planted a minimum of 5' from an underground utility, unless the city's Landscape Architect requires a greater distance. These requirements are incorporated into the plan.

CLOSING COMMENT:
Prior to the release of the performance bond, the City of Rochester Hills Forestry Division needs to inspect all trees, existing or planted, to identify any that pose a hazard to the safe use of the public right-of-way. Forestry may require the developer to remove, and possibly replace, any such trees.

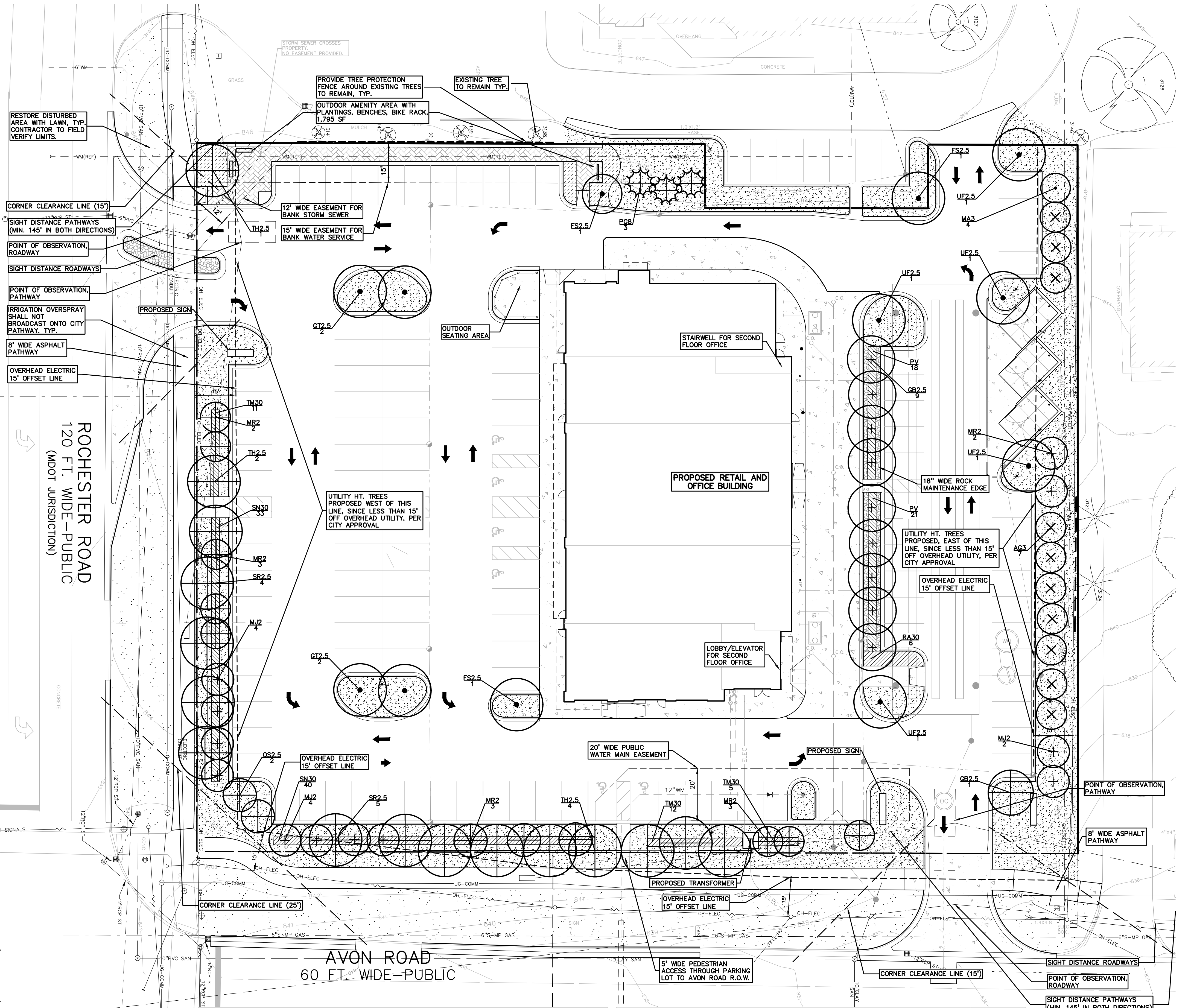
SECTION 138-12.109 - Maintenance

The owner of the property shall be responsible for all maintenance of site landscaping, as follows:

- Landscaping shall be kept in a neat, orderly and healthy growing condition, free from debris and refuse.
- Pruning shall be minimal at the time of installation, only to remove dead or diseased branches. Subsequent pruning shall assure proper maturation of plants to achieve their approved purpose.
- All dead, damaged, or diseased plant material shall be removed immediately and replaced within six months after it dies or in the next planting season, whichever occurs first. For purposes of this Section, the planting season for deciduous plants shall be between March 1 and June 1 and from October 1 until the prepared soil becomes frozen. The planting season for evergreen plants shall be between March 1 and June 1. Plant material installed to replace dead or diseased material shall be as close as practical to the size of the material it is intended to replace. The City may notify property owners of the need to replace dead, damaged, or diseased material.
- The approved landscape plan shall be considered a permanent record and integral part of the Site Plan Approval. Unless otherwise approved in accordance with the aforementioned procedures, any revisions to or removal of plant materials, or non-compliance with the maintenance requirements of this Section 138-12.109 will place the parcel in non-conformity with the approved landscape plan and be a violation of this ordinance.
- If protected trees are damaged, a fine shall be issued on an inch-by-inch basis at a monetary rate as defined by the Forestry Department.

NOT FOR CONSTRUCTION

XREF: S:\PROJECTS\2019\2019-007\DWG\TOP\BASE-19007.dwg
XREF: S:\PROJECTS\2019\2019-007\DWG\SITE PLANS\CBASE-19007.dwg
XREF: S:\PROJECTS\2019\2019-007\DWG\SITE PLANS\BLK-19007.dwg



EVERGREEN TREE PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
3	PG8	White Spruce	<i>Picea glauca</i>	8-10' Ht.	B&B
3	TOT.EVG.				

SHRUB PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
6	RA30	Gro Low Sumac	<i>Rhus aromatica 'Gro-Low'</i>	30" Ht.	Cont.
73	SN30	Neon Flash Spirea	<i>Spiraea japonica 'Neon Flash'</i>	30" Ht.	Cont.
28	TM30	Dense Yew	<i>Taxus x media 'Densiformis'</i>	30" Ht.	Cont.
107	TOT.SHRUB				

PERENNIAL PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
39	PV	Red Switch Grass	<i>Panicum virgatum 'Rotstrahbusch'</i>	1 Gal.	Cont.
39	TOT.PER.				

L-1.0 DECIDUOUS TREE PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
7	AG3	Autumn Brilliance Serviceberry	<i>Amelanchier x grandiflora 'Autumn Brilliance'single stem'</i>	3" Cal.	B&B
3	FS2.5	Columnar Beech	<i>Fagus sylvatica 'Fastigiata'</i>	2.5-3" Cal.	B&B
10	GB2.5	Fastigate Ginkgo (columnar)	<i>Ginkgo biloba 'Fastigiata'</i>	2.5-3" Cal.	B&B
4	GT2.5	Streetkeeper Honeylocust	<i>Gleditsia triacanthos 'Street keeper' Inermis'</i>	2.5-3" Cal.	B&B
4	MA3	Adirondack Crab	<i>Malus 'Adirondack' (10' x 18' columnar)</i>	3" Cal.	B&B
10	MJ2	Marilee Crab	<i>Malus 'Jardin' PP (10' x 20' columnar)</i>	2-2.5" Cal.	B&B
12	MR2	Red Baron Crab	<i>Malus 'Red Baron' (10' x 18' columnar)</i>	2-2.5" Cal.	B&B
2	QS2.5	Skinny Genes Oak	<i>Quercus alba 'JFS-KW2QX' P.A.F. (col./Fastigiata)</i>	2.5" Cal.	B&B
9	SR2.5	Japanese Tree Lilac	<i>Syringa reticulata, 15' x 20'</i>	2.5-3" Cal.	B&B
7	TH2.5	Summer Sprite Linden	<i>Tilia cordata 'Haika', 15' x 20'</i>	2.5-3" Cal.	B&B
5	UF2.5	Frontier Elm	<i>Ulmus 'Frontier'</i>	2.5-3" Cal.	B&B
73	TOT.DEC.				

S:\PROJECTS\2019\2019-007\445-5 ROCHESTER ROAD-PEA\DWG\SITE PLANS\L-1.0 LANDSCAPE-19007.dwg

issued for:
 OWNER REVIEW: 11 SEPT. 2020
 OWNER REVIEW: 01 FEB. 2021
 OWNER REVIEW: 10 FEB. 2021
 OWNER REVIEW: 11 FEB. 2021
 OWNER REVIEW: 08 MAR. 2021
 OWNER REVIEW: 19 APR. 2021
 OWNER REVIEW: 20 APR. 2021
 OWNER REVIEW: 22 APR. 2021
 SITE PLAN REVIEW: 05 MAY 2021

project:

Proposed
Commercial Development

NEC Rochester Rd. & Avon Rd.
 Rochester Hills, Michigan



32500 TELEGRAPH ROAD
 SUITE 250
 BINGHAM FARMS, MICHIGAN
 48025-2404
 PH 248.540.7700 FX 248.540.2710
 www.rogvoy.com



drawing:

**First Level
 Floor Plan**

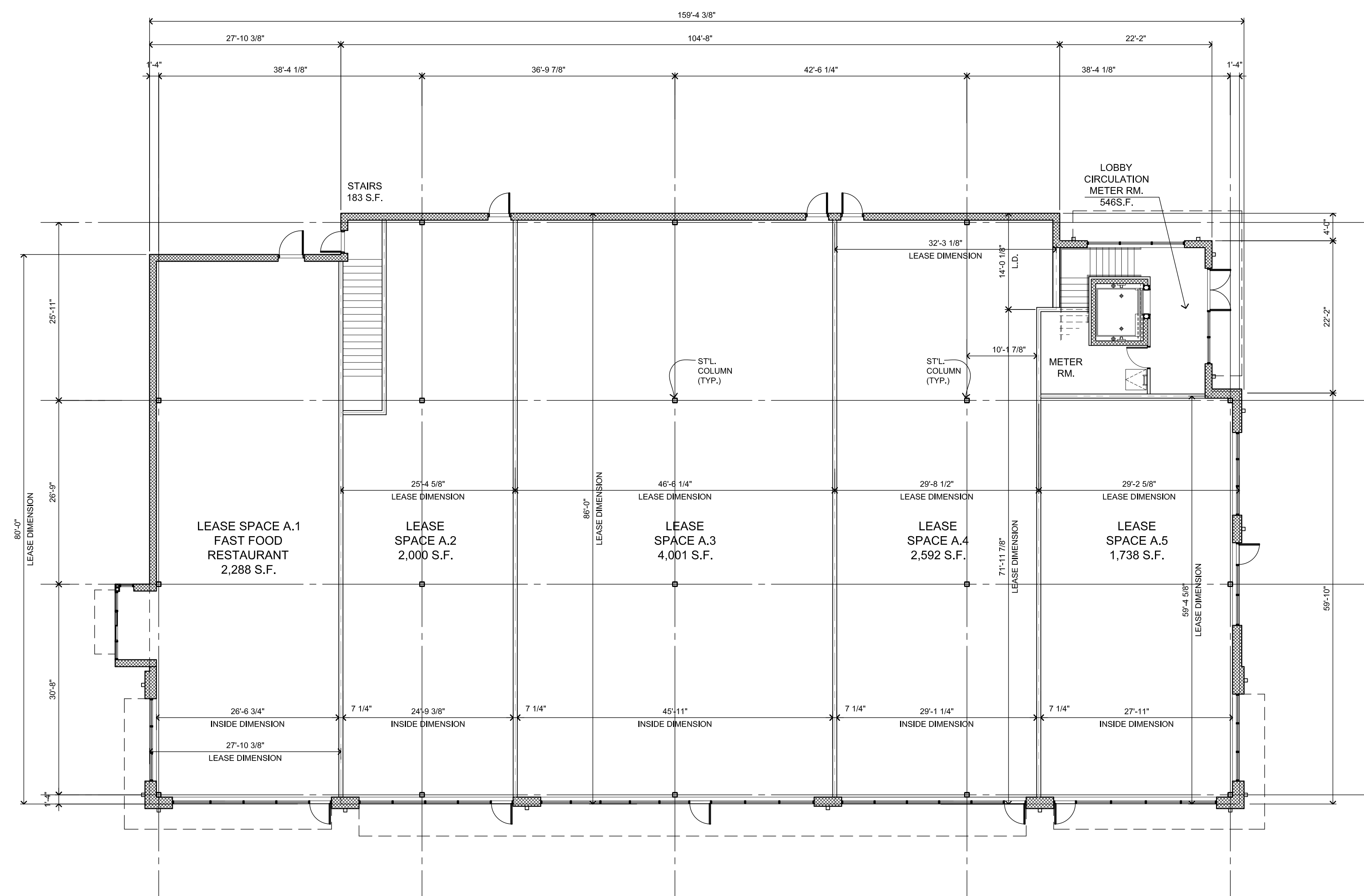
DO NOT SCALE DRAWING

issue date:
 drawn: KL
 checked: MD
 approved: MD

file number: 19026

sheet:

A-1



1st Level Floor Plan

GFA : 13,348 S.F.
 GLA : 12,619 S.F.

Scale: 3/32"=1'-0"

issued for:
 OWNER REVIEW: 11 SEPT. 2020
 OWNER REVIEW: 01 FEB. 2021
 OWNER REVIEW: 11 FEB. 2021
 SITE PLAN REVIEW: 05 MAY 2021

project:

Proposed
Commercial Development
 NEC Rochester Rd. & Avon Rd.
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32500 TELEGRAPH ROAD
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 48025-2404
 PH 248.540.7700 FX 248.540.2710
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drawing:
**Second Level
 Floor Plan**

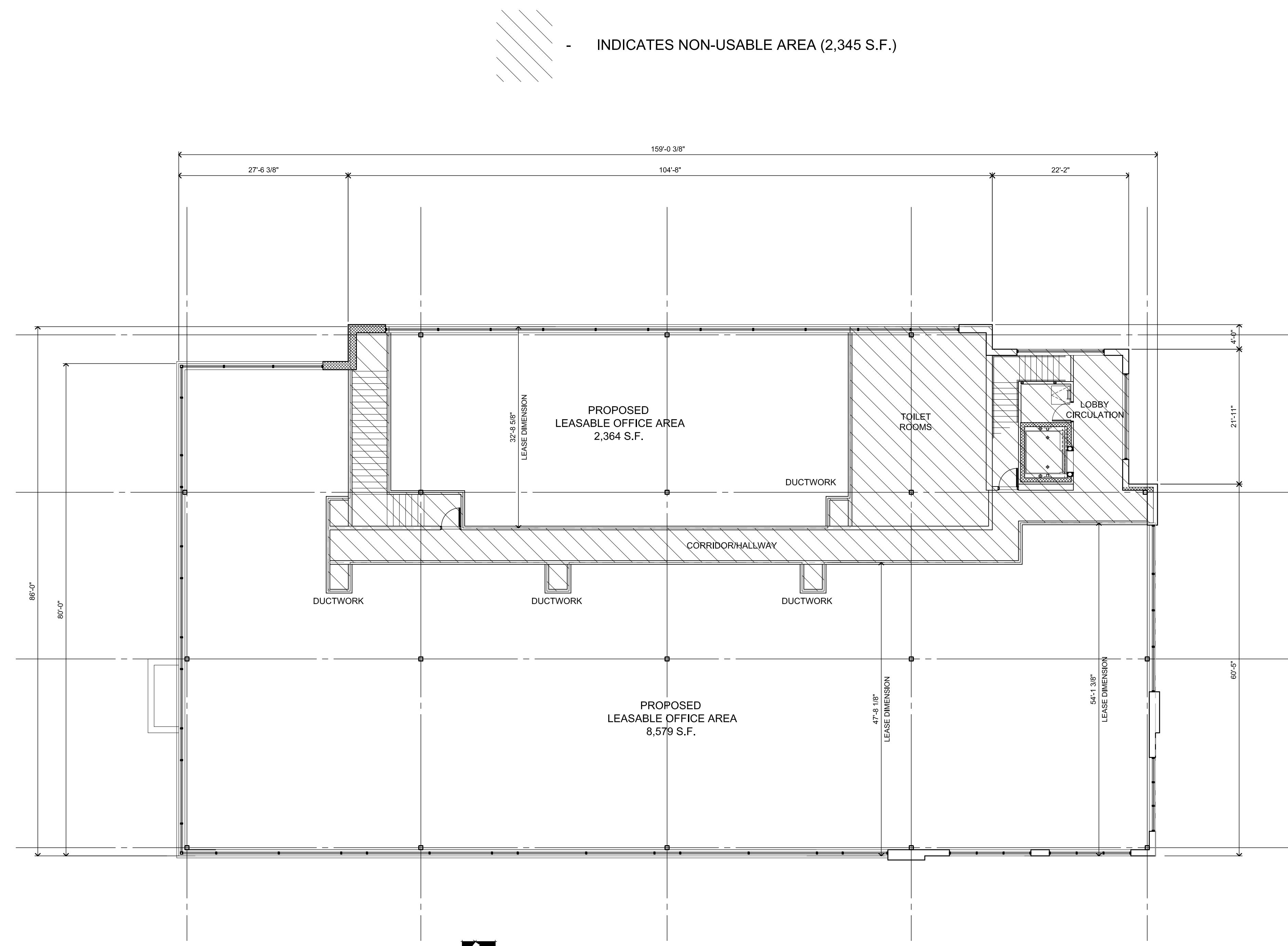
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issue date:
 drawn: KL
 checked: MD
 approved: MD

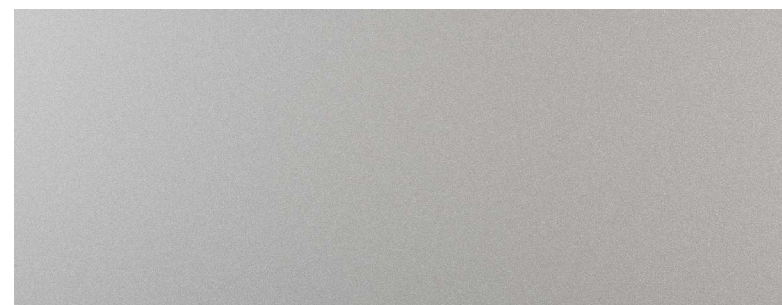
file number: **19026**

sheet:

A-2



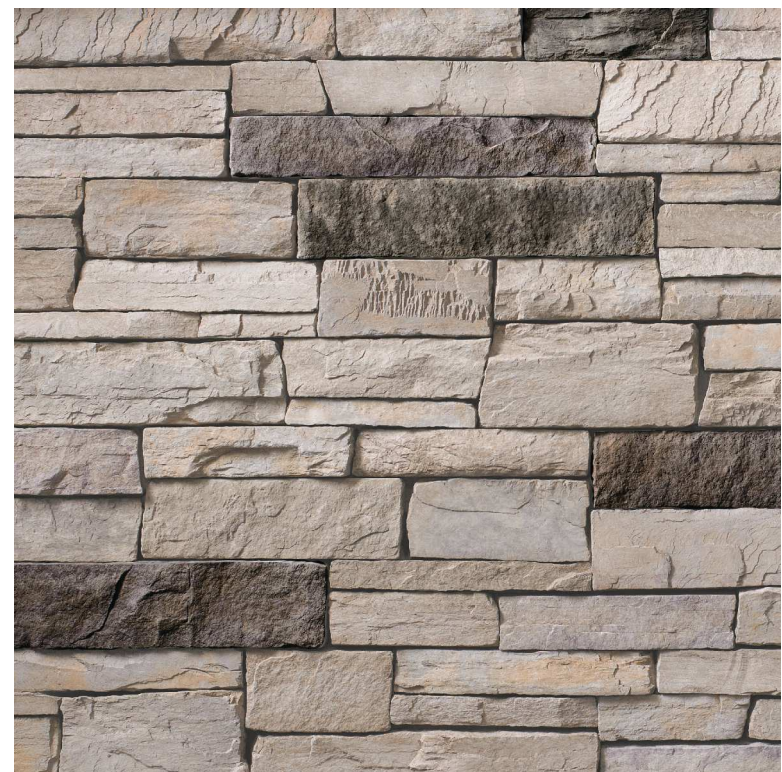
2nd Level Floor Plan
 GFA : 13,288 S.F.
 GLA : 10,943 S.F.
 Scale: 3/32"=1'-0"



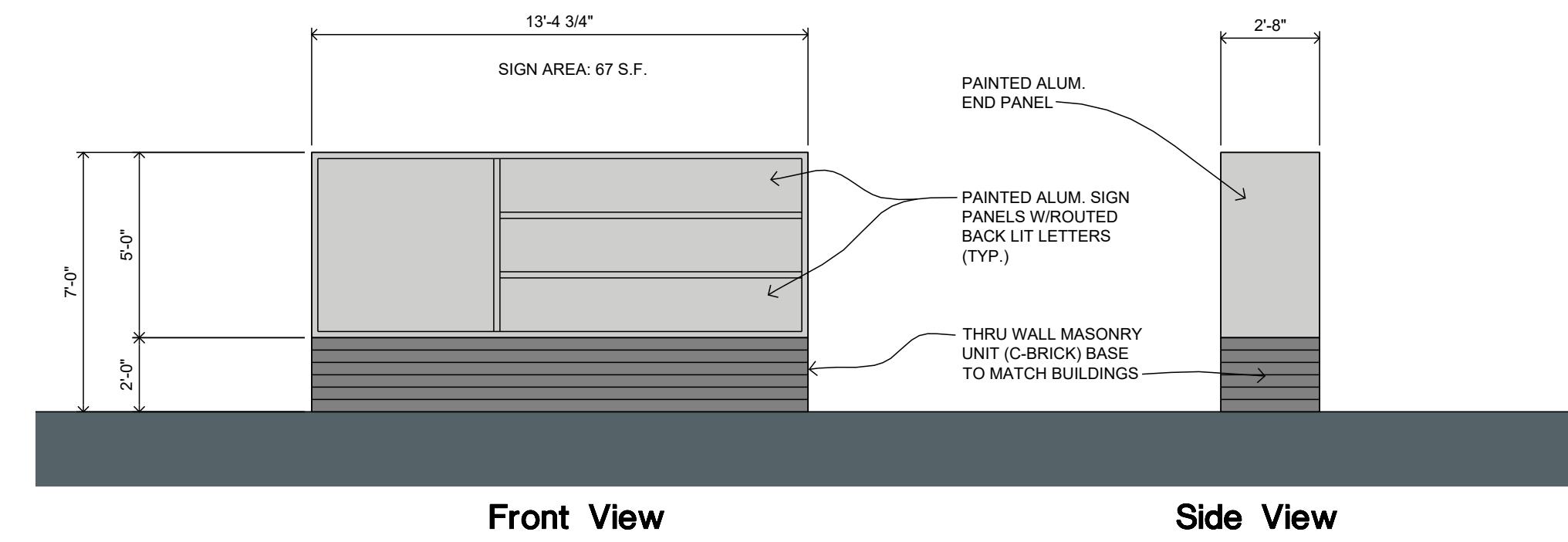
- ALUMINUM WALL PANEL : ALUCABOND COLOR: 'CHAMPAGNE METALLIC'



- BRICK : ALUCABOND COLOR: BRICK TECH ENDICOTT COLOR: 'MANGANESE IRON SPOT'

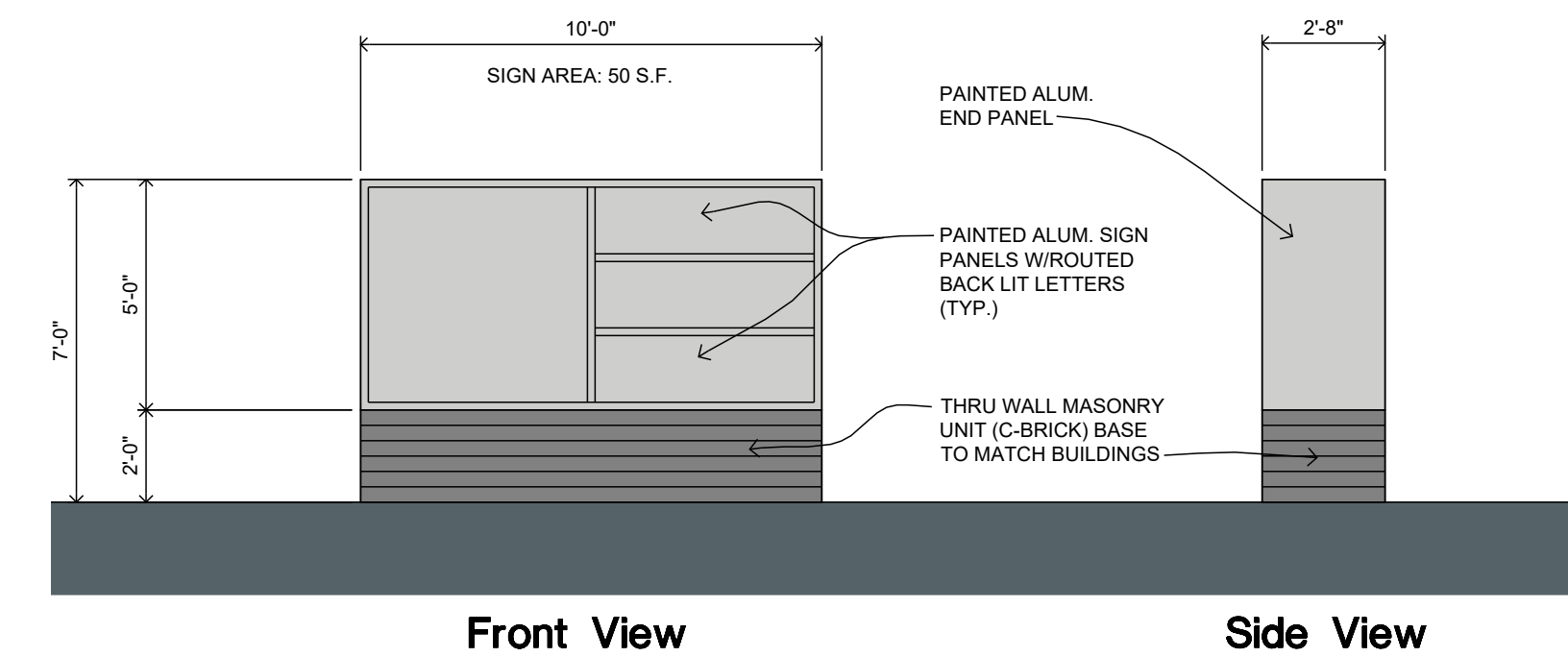


- STONE : CULTURED STONE COUNTRY LEDGESTONE COLOR: 'ASHFALL'



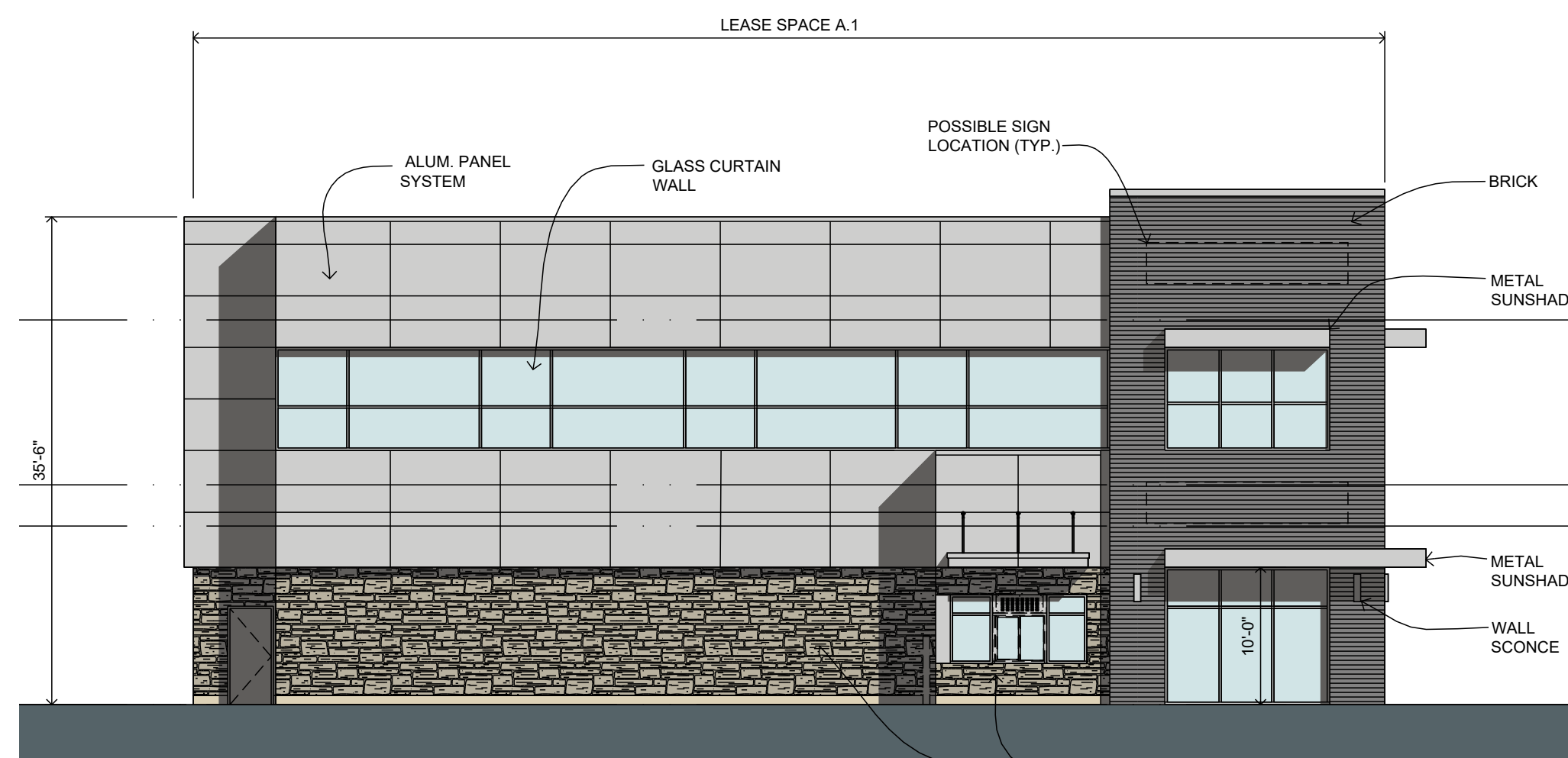
Monument Sign - Avon Rd.

Scale: 1/4"=1'-0"



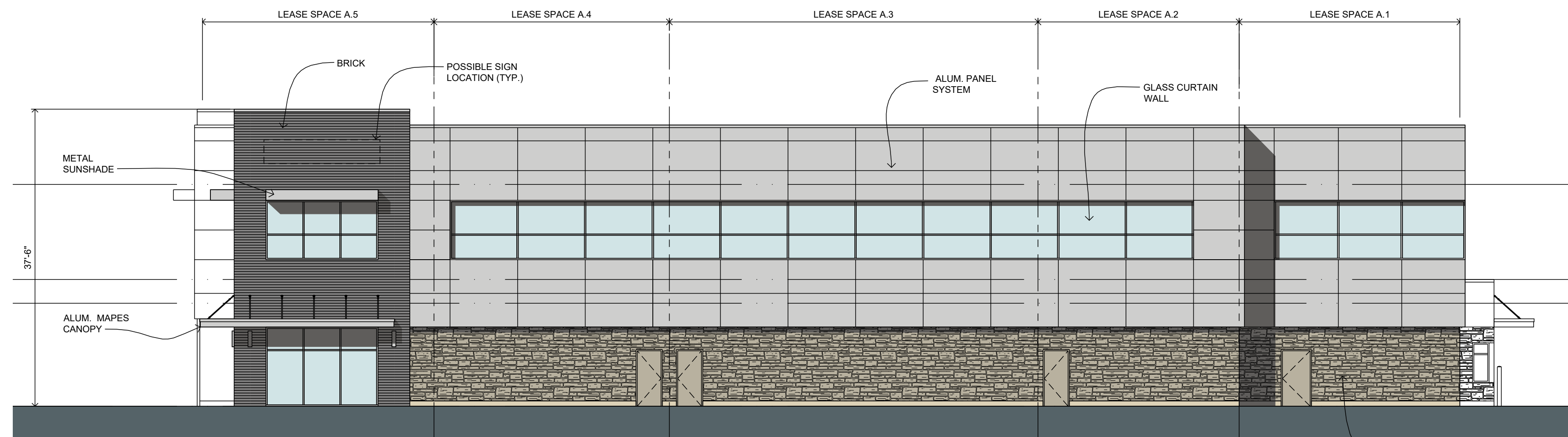
Monument Sign - Rochester Rd.

Scale: 1/4"=1'-0"



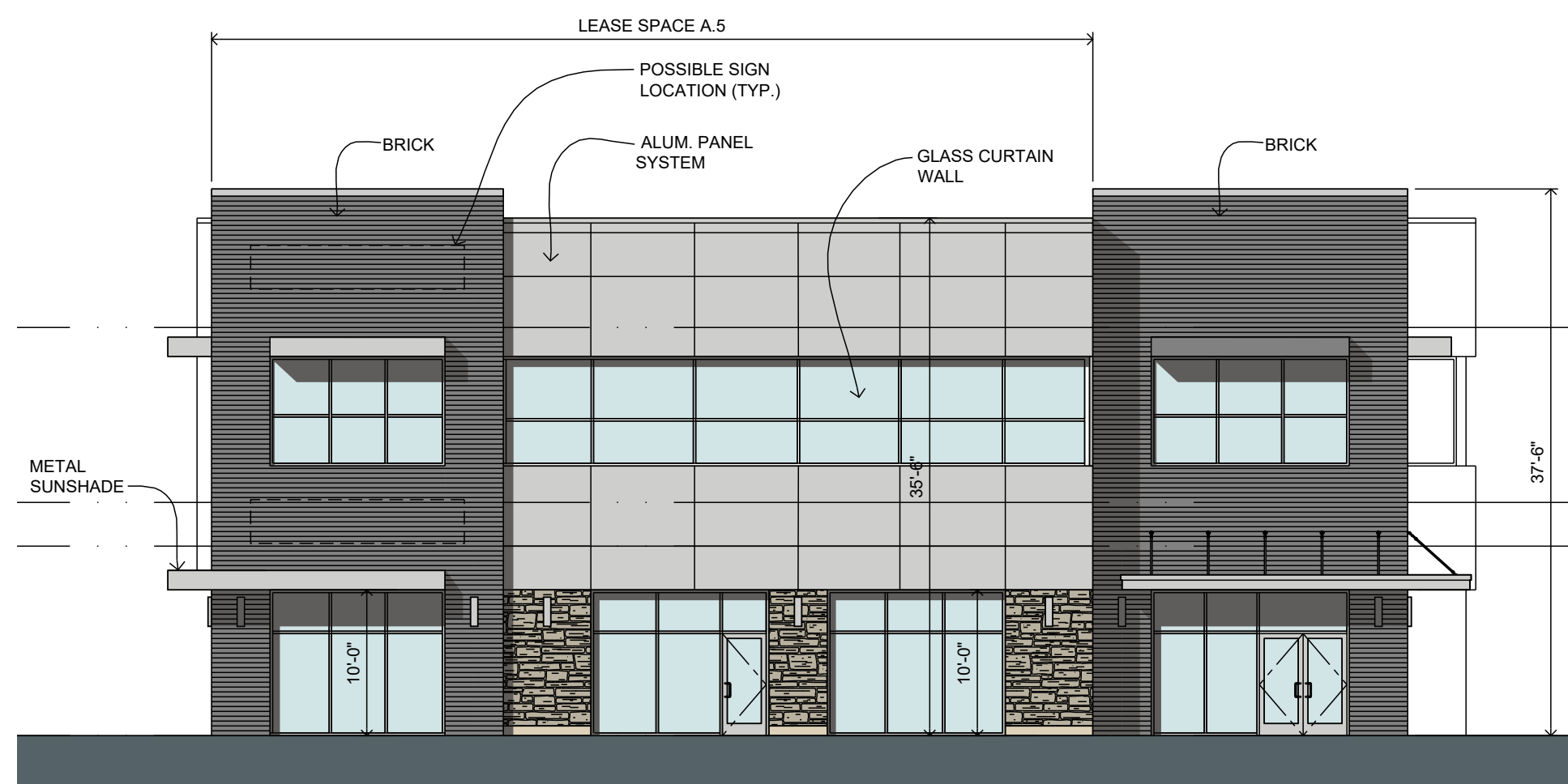
North Elevation

Scale: 3/32"=1'-0"



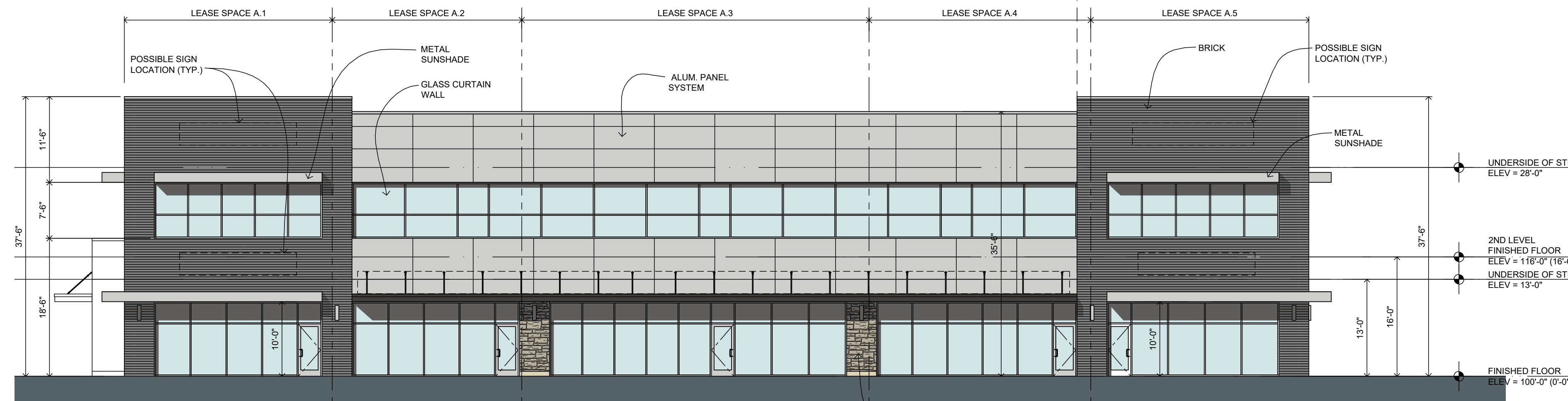
East Elevation

Scale: 3/32"=1'-0"



South Elevation

Scale: 3/32"=1'-0"



West Elevation

Scale: 3/32"=1'-0"

project:

Proposed
Commercial Development
 NEC Rochester Rd. & Avon Rd.
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drawing:
Conceptual Bldg. Elevations

DO NOT SCALE DRAWING

issue date:
 drawn: KL
 checked: MD
 approved: MD

file number: 19026

sheet:

Attachment D



Documentation of Eligibility

2019 Phase I Environmental Site Assessment (ESA) - PM Environmental, Inc. (PM)

PM conducted a Phase I ESA for the parcels located at 945 and 975 South Rochester Road on September 10, 2019. At the time of the Phase I, the subject property was occupied by the existing gasoline station and convenience store. There were no service operations observed during the Phase I.

Standard and historical sources documented the subject property consisted of agricultural and vacant land from at least 1937 until between 1964 and 1967 when the current building was constructed in the northeastern portion. The building was utilized for gasoline service station operations from at least 1967 until 1999, when the building was renovated into the current convenience store and gasoline dispensing station. A canopy was constructed in the western portion in 1980, which was replaced with the current canopy in 1997. The property has operated as a gasoline dispensing station since 1999.

PM reviewed the following reports pertaining to previous environmental investigations completed at the subject property.

Name of Report	Date of Report	Company that Prepared Report
Final Assessment Report	4-8-1997	EnecoTech Midwest, Inc. (ETM)
Groundwater Monitoring Report	7-28-1997	ETM
Groundwater Monitoring Report	10-8-1997	ETM
Groundwater Monitoring Report	1-22-2003	Groundwater & Environmental Services, Inc. (GES)
Closure Report	8-27-2004	GES
Phase I ESA	2-22-2005	AKT Peerless Environmental Services (AKT)
Subsurface Investigation Report	3-31-2005	AKT
Underground Storage Tank System Assessment and Closure Report	7-17-2008	Midwest Environmental Consulting Group (MECG)

The subject property, 975 South Rochester Road, is a closed Leaking Underground Storage Tank (LUST) site with two releases reported on April 8, 1996 (Leak ID: C-0214-96) and April 24, 1996 (Leak ID: C-0252-96) which were granted a Tier I Commercial III Restricted Closure on February 17, 2005.

Various site assessment activities were conducted between 1997 and 2008 which included the removal of several underground storage tanks (USTs) along with the excavation of approximately 40 cubic yards of soil; soil and groundwater sampling activities; and groundwater monitoring activities. Analytical results following the August 2004 Closure Report and March 2005 Subsurface Investigation Report documents groundwater sampling events which exceeded the current Part 213 Risk Based Screening Levels (RBSLs), and the current Recommended Interim Action Screening Levels (RIASLs) indicating a vapor intrusion concern may be present. Additionally, the March 2005 site investigation activities included a ground penetrating radar (GPR) survey to assess the potential for orphan USTs. The report indicates the GPR survey did not identify any anomalies consistent with the presence of orphan USTs, however, the GPR survey was not included within the report reviewed. Therefore, PM identified the lack of the GPR survey as a significant deficiency and the potential for orphan USTs as a recognized environmental condition (REC). Furthermore, the site investigation activities completed between 1997 and 2008 did not collect any groundwater or soil samples within the footprint of the subject

building. Therefore, the long-term historical automotive service operations have not been adequately assessed and was identified as a REC.

Based on the cumulative 1997-2008 analytical results, concentrations of various gasoline volatile organic compounds (VOCs) were identified in soil and groundwater exceeding the current Part 213 Residential and Nonresidential Risk Based Screening Levels (RBSLs). In addition, contaminant concentrations may be indicative of a vapor intrusion concern based on the separation distance and lack of delineation of contamination towards the subject building. Based on these analytical results and its closed LUST status, the subject property meets the definition of a "property," in accordance with Part 213 of P.A. 451 of the Michigan Natural Resources Environmental Protection Act (NREPA), as amended, which represents a REC).

The following onsite recognized RECs were identified in the Phase I ESA:

- The subject property, 975 South Rochester Road, operated as a gasoline dispensing station since between 1964 and 1967. Additionally, the subject property is a closed LUST site with two reported releases in 1996. The releases were granted a Restricted Tier I Commercial III Closure in February 2005. Review of previous site assessment activities documents that soil and groundwater contamination remains on-site above the current Part 213 RBSLs. In addition, contaminant concentrations may be indicative of a vapor intrusion concern. Based on these analytical results and its closed LUST status, the subject property meets the definition of a "property," in accordance with Part 213 of P.A. 451 of the Michigan Natural Resources Environmental Protection Act (NREPA), as amended.
- Review of previous site investigations document the current UST system was last assessed in August 2008. The subject property has continued to operate as a gasoline dispensing station with the current layout since August 2008 with no site assessment activities since that time. The potential exists that a new release has occurred from the current UST system and/or fuel dispensers.
- The subject property, 975 South Rochester Road, operated as a gasoline service station from at least 1967 until 1999. Historical interior waste streams associated with the former service operations would have consisted of general hazardous substances and/or petroleum products. The previous site investigation activates did not adequately assess all areas of the former gasoline service station. A significant portion of this time period preceded major environmental regulations and current waste management and disposal procedures. The historical waste management practices associated with the former gasoline service station operations are unknown and may be a source of subsurface contamination.
- The previous site investigation activates did not adequately assess the former gasoline service station area, which likely contained in-ground hoists. In-ground hoists have an underground reservoir for hydraulic fluids, which can contain polychlorinated biphenyls (PCBs). The potential exists that a release occurred from the former hydraulic hoist system and/or underground reservoir.
- The subject property, 975 South Rochester Road, operated as a gasoline dispensing and/or service station since between 1964 and 1967. PM was unable to obtain information

regarding the UST systems prior to and between 1967 and 1972. Therefore, additional fueling and/or new or waste oil USTs may have historically been present. Further, records indicate the subject buildings were connected to natural gas in 1978 and 1980, and the 975 South Rochester Road subject building likely utilized a fuel oil UST prior to natural gas connection. The potential exists for orphan USTs to be present on the property and/or for a release to have occurred.

A controlled recognized environmental condition (CREC), as defined in the ASTM Standard, is a REC resulting from a past release of hazardous substances or petroleum products that has been addressed to the satisfaction of the applicable regulatory authority with hazardous substances or petroleum products allowed to remain in place subject to the implementation of required controls. The following CREC was identified during completion of this Phase I ESA:

- The subject property located at 975 South Rochester Road is a closed LUST site with two releases reported in 1996. The release was granted a Restricted Tier I Commercial III Closure in February 2005. Review of previous site assessment activities documents that soil and groundwater contamination remains on-site above the current Part 213 Residential and Nonresidential RBSLs. In addition, contaminant concentrations may be indicative of a vapor intrusion concern. A deed restriction was recorded in 2004 that eliminates the use of groundwater for any purpose on the subject property, and any future change in the land use may necessitate further evaluation of potential risks to the public health, safety, and welfare to the environment. Based on the closed LUST status and restrictions associated with the LUST closure, PM has identified the closed LUST status as a CREC.

2019 Baseline Environmental Assessment (BEA) - PM Environmental, Inc.

The BEA was prepared and submitted for the parcel located at 975 S. South Rochester Road on behalf of Rochester Avon Partners, LLC on September 10, 2019, based on the available data documented in the Phase I. Based upon documented soil and groundwater exceedances of the Part 213 Residential and Nonresidential DWP/DW and GSIP/GSI RBSLs, and the closed LUST status, the subject property is a "Property" under Part 213 of P.A. 451, as amended and the rules promulgated thereunder.

2019 Documentation of Due Care Compliance (DDCC) - PM Environmental, Inc.

A DDCC report was prepared for the 975 S. Rochester Road parcel on November 19, 2019. The report documented the prior investigations and indicated that current and future use of the property would remain the same through January 2020. At that time, borrower intends to redevelop the property for commercial purposes. The subject property is currently zoned B-5: Automotive Business. The current and intended use and zoning is consistent with Nonresidential property use as defined under Part 213.

The DDCC took into account the following obligations:

- (a) Undertake measures as are necessary to prevent exacerbation.
- (b) Exercise due care by undertaking corrective action necessary to mitigate unacceptable exposure to regulated substances, mitigate fire and explosion hazards due to regulated

substances, and allow for the intended use of the property in a manner that protects the public health and safety.

- (c) Take reasonable precautions against the reasonably foreseeable acts or omissions of a third party and the consequences that foreseeably could result from those acts or omissions.
- (d) Provide reasonable cooperation, assistance, and access to the persons that are authorized to conduct corrective action activities at the property, including the cooperation and access necessary for the installation, integrity, operation, and maintenance of any complete or partial corrective action activity at the property.
- (e) Comply with any land use or resources use restrictions established or relied on in connection with the corrective action activities at the property.
- (f) Not impede the effectiveness or integrity of any land use or resource use restriction employed at the property in connection with corrective action activities.

Additional site evaluations were completed to support the DDCC. The scope of work included a ground penetrating radar (GPR) survey, six soil borings (SB-1 through SB-6), installing four temporary monitoring wells (TMW-3 through TMW-6) and collecting eight soil samples and four groundwater samples for laboratory analysis. The soil and groundwater samples were submitted for laboratory analysis of VOCs, polynuclear aromatics (PNAs), polychlorinated biphenyls, (PCBs), and metals (cadmium, chromium, and lead), or some combination thereof.

No anomalies consistent with the presence of orphan USTs or inground hoists were identified during the completion of the GPR survey. However, PM confirmed the presence of the previously closed in place USTs consisting of two 10,000-gallon gasoline UST and one 6,000-gallon gasoline UST in 2008 located south of the subject building.

The results of the additional evaluation indicated that concentrations of VOCs and PNAs were present in soil above one or more of Drinking Water Protection (DWP), Groundwater Surface Water Interface Protection (GSIP), Soil Volatilization to Indoor Air Inhalation (SVII) and Direct Content (DC) criteria at three of the six boring locations.

Concentrations of VOCs, PNAs and metals were detected in groundwater exceeding one or more of DWP and GSIP criteria in two of the four samples analyzed.

As such, the following response actions were defined:

Confirmed LUST Release

Based on the results of PM's September 2019 site investigation, a confirmed release (REL-0223-19) was reported for the subject property for Safeway Acquisitions Group LLC (i.e., the current operator) on October 3, 2019.

Notices

Due to the presence of contamination exceeding the Part 213 Nonresidential DC RBSLs, Light Non-Aqueous Phase Liquid (LNAPL) SVII and DC screening levels, written notices have been provided to easement holders of record, utility franchise holders of record, and the owners and/or operators of all public utilities that serve the subject property.

Preventing Building Occupant Vapor Intrusion Exposures

Concentrations of gasoline VOCs were identified in soil samples collected from the subject property in 2005 and 2019 above the Non-Aqueous Phase Liquid (NAPL) SVII screening levels. Soil gas sampling, and/or additional vapor intrusion evaluations and/or engineering controls will be necessary to mitigate inhalation exposure risks within the current and any future proposed buildings.

Direct Contact

Concentrations of gasoline VOCs indicative of the presence of residual LNAPL were identified in the soil samples collected from the southern portion subject property in the area of the former and current gasoline UST basins depths ranging between 3.0 and 4.0 feet bgs above the LNAPL DC screening levels. Additionally, concentrations of benzo(a)pyrene were identified in soil samples collected from the northern portion of the subject property in the area of the current gasoline/diesel UST basin and/or product piping depths ranging between 3.0-4.0 feet bgs above the Part 213 DC RBSL. Therefore, surface cover (i.e. existing asphalt and concrete, concrete building slab, gravel, and areas of seeded grass and topsoil) will be maintained to prevent dermal contact with contaminated soils.

These responsive actions are sufficient to prevent unacceptable exposure to hazardous substances for subject property occupants and onsite subsurface workers and comply with the requirements of Section 21304c of the NREPA. If changes to the property use, zoning, operations, and/or layout occur, re-evaluation of potential exposure pathways and associated amendments to this report will be required. In addition, activities to comply with Section 4c obligations are still required.

The DDCC also specified general notification and material handling/disposal recommendations in the event new construction or utility work was conducted on the property.

SB-2	
9/24/2019	
3.0 ~ 4.0'	
UNITS	ug/Kg
2-M	800
NAPH	2,000
OTHER VOCs	<MDL
ACE	1,400
ACETHY	800
ANT	5,500
B(a)ANTH	10,900
B(a)PYR	8,600
B(b)FLA	14,700
B(g,h,i)PER	3,200
B(k)FLA	15,500
CHRYSENE	10,600
FL	26,500
F	4,100
I(1,2,3-CD)PY	3,200
2-M	2,400
NAPH	6,400
Ph	26,600
Py	25,300
OTHER PNAs	<MDL

SB-1	
9/24/2019	
8.0 ~ 9.0'	
UNITS	ug/Kg
VOCs	<MDL
PNAs	<MDL

SB-5	
9/24/2019	
7.5 ~ 8.5'	
UNITS	ug/Kg
VOCs	<MDL
PNAs	<MDL
Cd	<MDL
Cr	3,560
Pb	4,100

SB-6	
9/24/2019	
1.0 ~ 2.0'	
UNITS	ug/Kg
B	70
OTHER VOCs	<MDL
PNAs	<MDL
Cd	<MDL
Cr	3,990
Pb	5,490

SB-3		SB-3	
9/24/2019		9/24/2019	
3.0 ~ 4.0'		12.0 ~ 13.0'	
UNITS	ug/Kg	UNITS	ug/Kg
BROMOFORM	20,000	VOCs	<MDL
E	16,000	PNAs	<MDL
2-M	40,000		
n-PROP	13,000		
1,2,3-TMB	49,000		
1,2,4-TMB	187,000		
1,3,5-TMB	86,000		
OTHER VOCs	<MDL		
2-M	18,300		
NAPH	16,700		
OTHER PNAs	<MDL		

SB-4		SB-4	
9/24/2019		9/24/2019	
3.0 ~ 4.0'		12.0 ~ 13.0'	
UNITS	ug/Kg	UNITS	ug/Kg
n-BUTYLB	4,900	VOCs	<MDL
E	700	PNAs	<MDL
2-M	15,000		
NAPH	10,000		
n-PROP	700		
1,2,3-TMB	12,000		
1,2,4-TMB	45,300		
1,3,5-TMB	19,100		
X	22,700		
OTHER VOCs	<MDL		
2-M	8,800		
NAPH	7,200		
OTHER PNAs	<MDL		

LEGEND:

- SUBJECT PROPERTY PARCEL / LOT BOUNDARIES
- APPROXIMATE FORMER/HISTORICAL SITE FEATURES
- STORM SEWER
- SANITARY SEWER
- GAS
- WATER
- OVERHEAD FIBER OPTIC
- UNDERGROUND COMMUNICATION LINE
- ELECTRIC
- OVERHEAD ELECTRIC LINE
- PROPOSED SITE FEATURES
- ⊕ POLE MOUNTED TRANSFORMER
- ⊕ FUEL DISPENSER
- ⊕ FORMER FUEL DISPENSER
- ⊕ VENT PIPE
- ⊕ UTILITY POLE
- ⊕ LIGHT POLE
- ⊕ DUMPSTER
- ⊕ CATCH BASIN
- ⊕ SQUARE CATCH BASIN
- ⊕ SANITARY MAN HOLE
- ⊕ UST
- ⊕ UNDERGROUND STORAGE TANK
- ⊕ SOIL BORING
- ⊕ SOIL BORING / TEMPORARY MONITORING WELL
- ⊕ PROPOSED MONITORING WELL
- ⊕ SOIL GAS
- ⊕ MONITORING WELL
- ⊕ MONITORING WELL / SOIL GAS SAMPLE
- ⊕ SOIL GAS SAMPLE
- ⊕ SOIL BORING / TEMPORARY MONITORING WELL / SOIL GAS SAMPLE
- ⊕ SOIL GAS SAMPLE
- ⊕ UST
- ⊕ UNDERGROUND STORAGE TANK
- Cd CADMIUM
- Cr CHROMIUM
- Pb LEAD
- E ETHYLBENZENE
- X XYLENES
- F FLUORENE
- FL FLUORANTHRENE
- Ph PHENANTHRENE
- Py PYRENE
- 2-M 2-METHYLNAPHTHALENE
- NAPH NAPHTHALENE
- ACE ACENAPHTHENE
- ACETHY ACENAPHTHYLENE
- ANT ANTHRACENE
- B(a)ANTH BENZO(a)ANTHRACENE
- B(a)PYR BENZO(a)PYRENE
- B(b)FLA BENZO(b)FLUORANTHRENE
- B(g,h,i)PER BENZO(g,h,i)PERYLENE
- B(k)FLA BENZO(k)FLUORANTHRENE
- I(1,2,3-CD)PY INDENO(1,2,3-CD)PYRENE
- n-PROP n-PROPYLBENZENE
- n-BUTYLB n-BUTYLBENZENE
- 1,2,4-TMB 1,2,4-TRIMETHYLBENZENE
- 1,3,5-TMB 1,3,5-TRIMETHYLBENZENE
- 1,2,3-TMB 1,2,3-TRIMETHYLBENZENE
- VOCs VOLATILE ORGANIC COMPOUNDS
- PNAs POLYNUCLEAR AROMATIC COMPOUNDS
- MDL METHOD DETECTION LIMIT
- SOIL CONCENTRATIONS REPRESENTATIVE OF POTENTIAL RESIDUAL LNAPL SATURATION
- REFER TO TABLES FOR SPECIFIC COMPOUNDS ANALYZED
- APPROXIMATE EXTENT OF SOIL IMPACT EXCEEDING PART 213 RBLS
- APPROXIMATE EXTENT OF RESIDUAL LNAPL

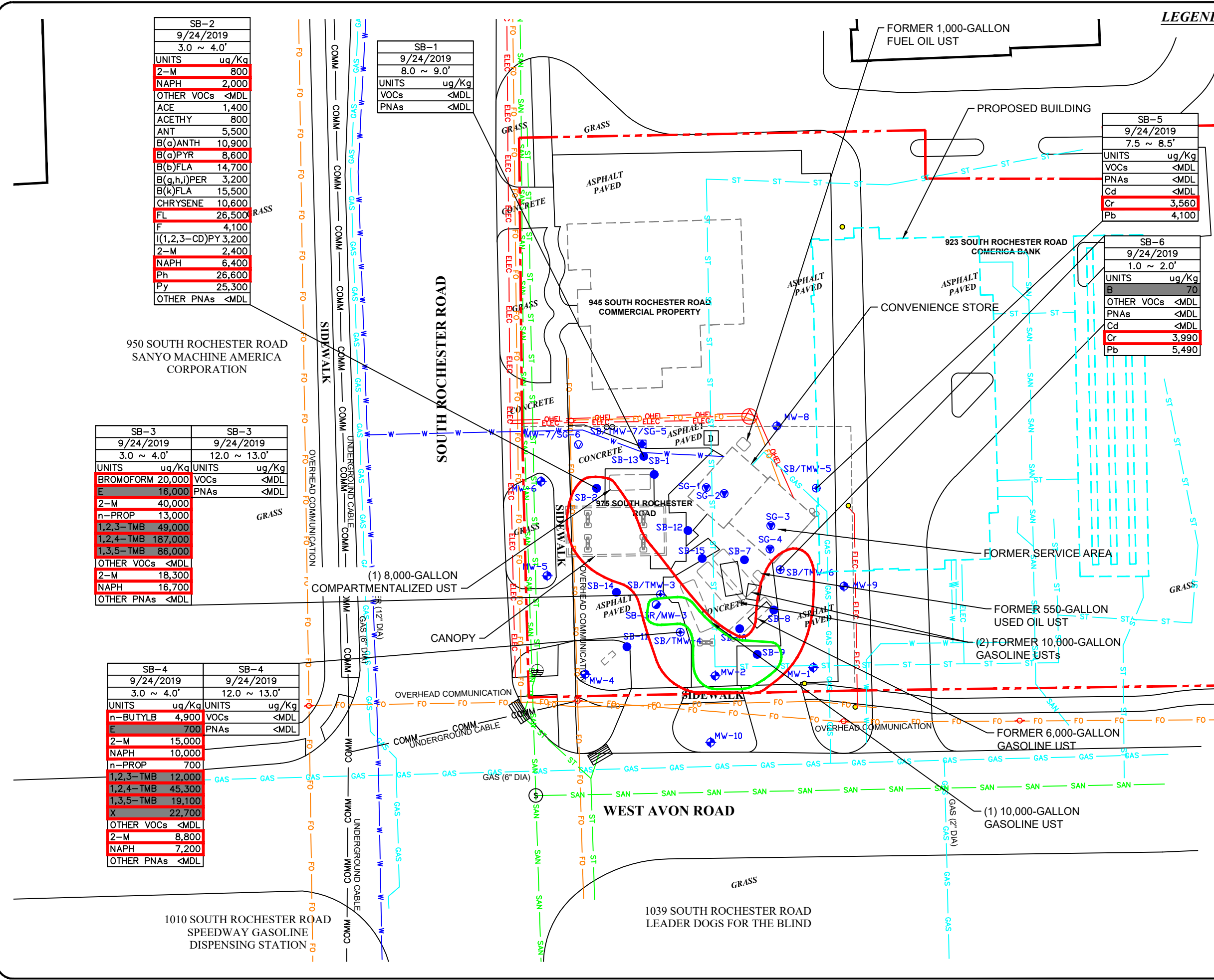
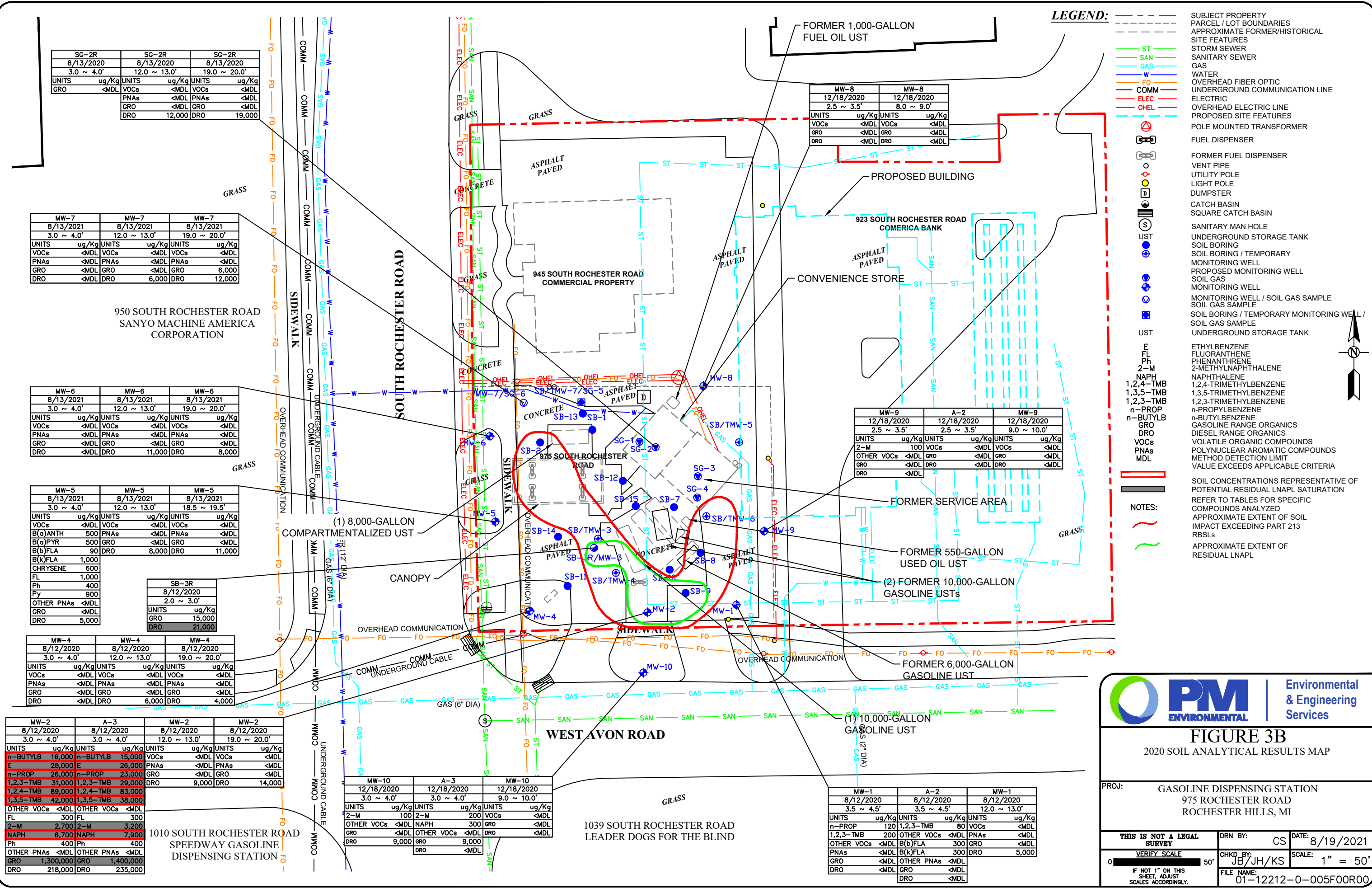


FIGURE 3A
2019 SOIL ANALYTICAL RESULTS MAP

PROJ:	GASOLINE DISPENSING STATION 975 ROCHESTER ROAD ROCHESTER HILLS, MI		
THIS IS NOT A LEGAL SURVEY	DRN BY:	CS	DATE: 8/19/2021
VERIFY SCALE	CHKO BY:	JB/JH/KS	SCALE: 1" = 50'
IF NOT 1" ON THIS SHEET, ADJUST SCALES ACCORDINGLY.	FILE NAME:	01-12212-0-005F00R00	



LEGEND:

- SUBJECT PROPERTY PARCEL / LOT BOUNDARIES
- APPROXIMATE FORMER/HISTORICAL SITE FEATURES
- ST STORM SEWER
- SAN SANITARY SEWER
- GAS GAS
- W WATER
- FO OVERHEAD FIBER OPTIC
- COMM UNDERGROUND COMMUNICATION LINE
- ELEC ELECTRIC
- OHEL OVERHEAD ELECTRIC LINE
- PROPOSED SITE FEATURES
- POLE MOUNTED TRANSFORMER
- FUEL DISPENSER
- FORMER FUEL DISPENSER
- VENT PIPE
- UTILITY POLE
- LIGHT POLE
- DUMPSTER
- CATCH BASIN
- SQUARE CATCH BASIN
- SANITARY MAN HOLE
- UNDERGROUND STORAGE TANK
- SOIL BORING
- SOIL BORING / TEMPORARY MONITORING WELL
- MONITORING WELL
- PROPOSED MONITORING WELL
- SOIL GAS MONITORING WELL
- MONITORING WELL / SOIL GAS SAMPLE
- SOIL GAS SAMPLE
- SOIL BORING / TEMPORARY MONITORING WELL / SOIL GAS SAMPLE
- UNDERGROUND STORAGE TANK
- E ETHYLBENZENE
- FL FLUORANTHRENE
- Ph PHENANTHRENE
- 2-M 2-METHYLNAPHTHALENE
- NAP NAPHTHALENE
- 1,2,4-TMB 1,2,4-TRIMETHYLBENZENE
- 1,3,5-TMB 1,3,5-TRIMETHYLBENZENE
- 1,2,3-TMB 1,2,3-TRIMETHYLBENZENE
- n-PROP n-PROPYLBENZENE
- n-BUTYLB n-BUTYLBENZENE
- GRO GASOLINE RANGE ORGANICS
- DRO DIESEL RANGE ORGANICS
- VOCs VOLATILE ORGANIC COMPOUNDS
- PNAs POLYNUCLEAR AROMATIC COMPOUNDS
- MDL METHOD DETECTION LIMIT
- VALUE EXCEEDS APPLICABLE CRITERIA

NOTES:

- SOIL CONCENTRATIONS REPRESENTATIVE OF POTENTIAL RESIDUAL LNAPL SATURATION
- REFER TO TABLES FOR SPECIFIC COMPOUNDS ANALYZED
- APPROXIMATE EXTENT OF SOIL IMPACT EXCEEDING PART 213 RBLSs
- APPROXIMATE EXTENT OF RESIDUAL LNAPL



FIGURE 3B
2020 SOIL ANALYTICAL RESULTS MAP

PROJ:	GASOLINE DISPENSING STATION 975 ROCHESTER ROAD ROCHESTER HILLS, MI		
THIS IS NOT A LEGAL SURVEY	DRN BY:	CS	DATE: 8/19/2021
VERIFY SCALE	CHKO BY:	JB/JH/KS	SCALE: 1" = 50'
IF NOT 1" ON THIS SHEET, ADJUST SCALES ACCORDINGLY.	FILE NAME:	01-12212-0-005F00R00	

SG-2R	SG-2R	SG-2R
8/13/2020	8/13/2020	8/13/2020
3.0 ~ 4.0'	12.0 ~ 13.0'	19.0 ~ 20.0'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
GRO <MDL	VOCs <MDL	VOCs <MDL
PNAs <MDL	PNAs <MDL	PNAs <MDL
GRO <MDL	GRO <MDL	GRO <MDL
DRO <MDL	DRO 12,000	DRO 19,000

MW-7	MW-7	MW-7
8/13/2021	8/13/2021	8/13/2021
3.0 ~ 4.0'	12.0 ~ 13.0'	19.0 ~ 20.0'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
VOCs <MDL	VOCs <MDL	VOCs <MDL
PNAs <MDL	PNAs <MDL	PNAs <MDL
GRO <MDL	GRO <MDL	GRO 6,000
DRO <MDL	DRO 6,000	DRO 12,000

MW-6	MW-6	MW-6
8/13/2021	8/13/2021	8/13/2021
3.0 ~ 4.0'	12.0 ~ 13.0'	19.0 ~ 20.0'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
VOCs <MDL	VOCs <MDL	VOCs <MDL
PNAs <MDL	PNAs <MDL	PNAs <MDL
GRO <MDL	GRO <MDL	GRO <MDL
DRO <MDL	DRO 11,000	DRO 8,000

MW-5	MW-5	MW-5
8/13/2021	8/13/2021	8/13/2021
3.0 ~ 4.0'	12.0 ~ 13.0'	18.5 ~ 19.5'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
VOCs <MDL	VOCs <MDL	VOCs <MDL
B(g)ANTH 500	PNAs <MDL	PNAs <MDL
B(c)PYR 500	GRO <MDL	GRO <MDL
B(b)FLA 90	DRO 8,000	DRO 11,000
B(k)FLA 1,000		
CHRYSENE 600		
FL 1,000		
Ph 400		
Py 900		
OTHER PNAs <MDL		
GRO <MDL		
DRO 5,000		

SB-3R
8/12/2020
2.0 ~ 3.0'
UNITS ug/Kg
GRO 15,000
DRO 21,000

MW-4	MW-4	MW-4
8/12/2020	8/12/2020	8/12/2020
3.0 ~ 4.0'	12.0 ~ 13.0'	19.0 ~ 20.0'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
VOCs <MDL	VOCs <MDL	VOCs <MDL
PNAs <MDL	PNAs <MDL	PNAs <MDL
GRO <MDL	GRO <MDL	GRO <MDL
DRO <MDL	DRO 6,000	DRO 4,000

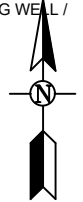
MW-2	A-3	MW-2	MW-2
8/12/2020	8/12/2020	8/12/2020	8/12/2020
3.0 ~ 4.0'	3.0 ~ 4.0'	12.0 ~ 13.0'	19.0 ~ 20.0'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
n-BUTYLB 16,000	n-BUTYLB 15,000	VOCs <MDL	VOCs <MDL
E 28,000	E 26,000	PNAs <MDL	PNAs <MDL
n-PROP 26,000	n-PROP 23,000	GRO <MDL	GRO <MDL
1,2,3-TMB 31,000	1,2,3-TMB 29,000	DRO 9,000	DRO 14,000
1,2,4-TMB 89,000	1,2,4-TMB 83,000		
1,3,5-TMB 42,000	1,3,5-TMB 38,000		
OTHER VOCs <MDL	OTHER VOCs <MDL		
FL 300	FL 300		
2-M 2,700	2-M 3,200		
NAPH 6,700	NAPH 7,900		
Ph 400	Ph 400		
OTHER PNAs <MDL	OTHER PNAs <MDL		
GRO 1,300,000	GRO 1,400,000		
DRO 218,000	DRO 235,000		

MW-10	A-3	MW-10
12/18/2020	12/18/2020	12/18/2020
3.0 ~ 4.0'	3.0 ~ 4.0'	9.0 ~ 10.0'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
2-M 100	2-M 200	VOCs <MDL
OTHER VOCs <MDL	NAPH 300	GRO <MDL
GRO 9,000	OTHER VOCs <MDL	DRO <MDL
DRO 9,000	DRO 9,000	DRO <MDL

MW-1	A-2	MW-1
8/12/2020	8/12/2020	8/12/2020
3.5 ~ 4.5'	3.5 ~ 4.5'	12.0 ~ 13.0'
UNITS ug/Kg	UNITS ug/Kg	UNITS ug/Kg
n-PROP 120	1,2,3-TMB 80	VOCs <MDL
1,2,3-TMB 200	OTHER VOCs <MDL	PNAs <MDL
OTHER VOCs <MDL	B(b)FLA 300	GRO <MDL
PNAs <MDL	B(k)FLA 300	DRO 5,000
GRO <MDL	OTHER PNAs <MDL	
DRO <MDL	GRO <MDL	
DRO <MDL	DRO <MDL	

LEGEND:

- SUBJECT PROPERTY PARCEL / LOT BOUNDARIES
- APPROXIMATE FORMER/HISTORICAL SITE FEATURES
- ST STORM SEWER
- SAN SANITARY SEWER
- GAS GAS
- W WATER
- FO OVERHEAD FIBER OPTIC
- COMM UNDERGROUND COMMUNICATION LINE
- ELEC ELECTRIC
- OHEL OVERHEAD ELECTRIC LINE
- PROPOSED SITE FEATURES
- POLE MOUNTED TRANSFORMER
- FUEL DISPENSER
- FORMER FUEL DISPENSER
- VENT PIPE
- UTILITY POLE
- LIGHT POLE
- DUMPSTER
- CATCH BASIN
- SQUARE CATCH BASIN
- SANITARY MAN HOLE
- UNDERGROUND STORAGE TANK
- SOIL BORING
- MONITORING WELL
- PROPOSED MONITORING WELL
- SOIL GAS MONITORING WELL
- MONITORING WELL / SOIL GAS SAMPLE
- SOIL GAS SAMPLE
- SOIL BORING / TEMPORARY MONITORING WELL / SOIL GAS SAMPLE
- UNDERGROUND STORAGE TANK
- B BENZENE
- T TOLUENE
- E ETHYLBENZENE
- X XYLENES
- 2-M 2-METHYLNAPHTHALENE
- NAPH NAPHTHALENE
- 1,2,4-TMB 1,2,4-TRIMETHYLBENZENE
- 1,3,5-TMB 1,3,5-TRIMETHYLBENZENE
- 1,2,3-TMB 1,2,3-TRIMETHYLBENZENE
- n-PROP n-PROPYLBENZENE
- GRO GASOLINE RANGE ORGANICS
- DRO DIESEL RANGE ORGANICS
- VOCs VOLATILE ORGANIC COMPOUNDS
- PNAs POLYNUCLEAR AROMATIC COMPOUNDS
- MDL METHOD DETECTION LIMIT
- SOIL CONCENTRATIONS REPRESENTATIVE OF POTENTIAL RESIDUAL LNAPL SATURATION
- REFER TO TABLES FOR SPECIFIC COMPOUNDS ANALYZED
- APPROXIMATE EXTENT OF SOIL IMPACT EXCEEDING PART 213 RBLSs
- APPROXIMATE EXTENT OF RESIDUAL LNAPL



SB-13		SB-13	
8/10/2021		8/10/2021	
3.0 ~ 4.0'		7.0 ~ 8.0'	
UNITS	ug/Kg	UNITS	ug/Kg
VOCs	<MDL	VOCs	<MDL
PNAs	<MDL	PNAs	<MDL
GRO	<MDL	GRO	<MDL
DRO	<MDL	DRO	<MDL

SB-12		SB-12	
8/10/2021		8/10/2021	
3.0 ~ 4.0'		9.0 ~ 10.0'	
UNITS	ug/Kg	UNITS	ug/Kg
VOCs	<MDL	VOCs	<MDL
PNAs	<MDL	PNAs	<MDL
GRO	<MDL	GRO	<MDL
DRO	<MDL	DRO	8,000

SB-15		SB-15	
8/10/2021		8/10/2021	
4.0 ~ 5.0'		8.0 ~ 9.0'	
UNITS	ug/Kg	UNITS	ug/Kg
VOCs	<MDL	VOCs	<MDL
PNAs	<MDL	PNAs	<MDL
GRO	<MDL	GRO	<MDL
DRO	<MDL	DRO	<MDL

SB-14		SB-14	
8/10/2021		8/10/2021	
4.0 ~ 5.0'		8.0 ~ 9.0'	
UNITS	ug/Kg	UNITS	ug/Kg
VOCs	<MDL	VOCs	<MDL
PNAs	<MDL	PNAs	<MDL
GRO	<MDL	GRO	<MDL
DRO	<MDL	DRO	<MDL

SB-8		A-2		SB-8	
8/9/2021		8/9/2021		8/9/2021	
5.0 ~ 6.0'		5.0 ~ 6.0'		7.5 ~ 8.5'	
UNITS	ug/Kg	UNITS	ug/Kg	UNITS	ug/Kg
B	300	B	420	VOCs	<MDL
2-M	400	2-M	500	PNAs	<MDL
NAPH	800	NAPH	1,100	GRO	<MDL
1,2,3-TMB	210	1,2,3-TMB	530	DRO	<MDL
1,2,4-TMB	600	1,2,4-TMB	1,610		
1,3,5-TMB	240	1,3,5-TMB	670		
X	400	X	1,800		
OTHER VOCs	<MDL	OTHER VOCs	<MDL		
2-M	700	2-M	500		
NAPH	900	NAPH	500		
OTHER PNAs	<MDL	OTHER PNAs	<MDL		
GRO	<MDL	GRO	24,000		
DRO	9,000	DRO	9,000		

SB-11		SB-11		A-3	
8/9/2021		8/9/2021		8/9/2021	
4.0 ~ 5.0'		9.5 ~ 10.5'		9.5 ~ 10.5'	
UNITS	ug/Kg	UNITS	ug/Kg	UNITS	ug/Kg
VOCs	<MDL	VOCs	<MDL	VOCs	<MDL
PNAs	<MDL	PNAs	<MDL	PNAs	<MDL
GRO	<MDL	GRO	<MDL	GRO	<MDL
DRO	<MDL	DRO	<MDL	DRO	<MDL

SB-9		SB-9	
8/9/2021		8/9/2021	
5.0 ~ 6.0'		10.0 ~ 11.0'	
UNITS	ug/Kg	UNITS	ug/Kg
B	36,000	B	80
E	96,000	E	300
2-M	20,000	2-M	200
n-PROP	44,000	n-PROP	180
T	648,000	T	1,800
1,2,3-TMB	52,000	1,2,3-TMB	220
1,2,4-TMB	212,000	1,2,4-TMB	890
1,3,5-TMB	73,000	1,3,5-TMB	310
X	613,000	X	1,960
OTHER VOCs	<MDL	OTHER VOCs	<MDL
2-M	500	PNAs	<MDL
NAPH	500	GRO	36,000
OTHER PNAs	<MDL	DRO	9,000
GRO	6,600,000		
DRO	536,000		

SB-7		SB-7	
8/9/2021		8/9/2021	
4.0 ~ 0'		4.0 ~ 0'	
UNITS	ug/Kg	UNITS	ug/Kg
VOCs	<MDL	VOCs	<MDL
PNAs	<MDL	PNAs	<MDL
GRO	10,000	GRO	<MDL
DRO	5,000	DRO	<MDL

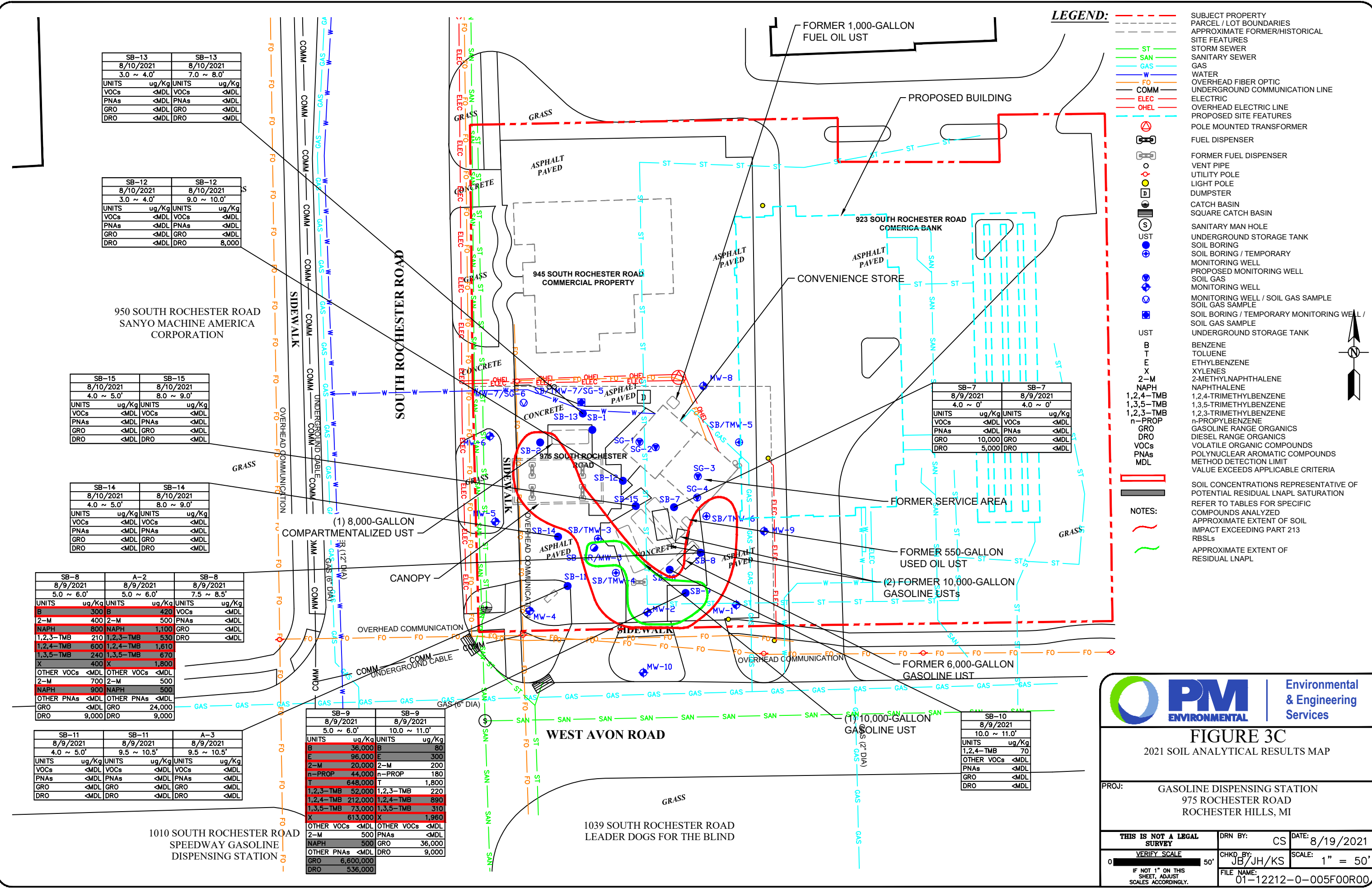


FIGURE 3C
2021 SOIL ANALYTICAL RESULTS MAP

PROJ: GASOLINE DISPENSING STATION
975 ROCHESTER ROAD
ROCHESTER HILLS, MI

THIS IS NOT A LEGAL SURVEY	DRN BY: CS	DATE: 8/19/2021
VERIFY SCALE	CHKD BY: JB/JH/KS	SCALE: 1" = 50'
IF NOT 1" ON THIS SHEET, ADJUST SCALES ACCORDINGLY.	FILE NAME: 01-12212-0-005F00R00	

Tables

Table 1: Eligible Activities Cost Estimates

Item/Activity	Total Local Only Eligible Activities
Pre-Approved Activities	
Phase I ESA	\$ 2,500
Phase II ESA/BEA/DDCC	\$ 10,800
Hazardous Materials Survey	\$ 2,025
Pre-Approved Activities Sub-Total	\$ 15,325
Department Specific Activities	
Vapor Barrier Design	\$ 20,000
Vapor Barrier Installation	\$ 60,100
Contaminated Soil Transport and Disposal	\$ 40,000
Cwater Treatment and Disposal - Groundwater Management	\$ 20,000
Frac Tank Rental - Groundwater Management	\$ 5,250
Utility Corridor Migration Barriers	\$ 5,000
Utility Gasketing	\$ 15,000
Underground Storage Tank Removal	\$ 32,000
Oversight, Sampling and Reporting by Environmental Professional	\$ 78,500
Department Specific Activities Sub-Total	\$ 275,850
Demolition	
Building Demolition Activities	\$ 60,000
Site Demolition Activities	\$ 60,000
Demolition Sub-Total	\$ 120,000
Asbestos and Lead Activities	
Asbestos Abatement	\$ 20,000
Asbestos and Lead Activities Sub-Total	\$ 20,000
Preparation of Brownfield Plan and Act 381 Workplan	
Brownfield Plan Preparation and Implementation	\$ 15,000
Brownfield Plan and Act 381 Workplan Sub-Total	\$ 15,000
Eligible Activities Sub-Total	\$ 446,175
15% Contingency*	\$ 62,378
Developer Eligible Reimbursement Total	\$ 508,553
TIF Capture for Local Brownfield Revolving Fund	\$ 183,318
Administrative Fee	\$ 36,414
Total	\$ 728,284

*15% Contingency excludes preparation of Brownfield Plan and Pre-Approved Activities

Table 2
Tax Increment Revenue Capture Estimates
945 and 975 S. Rochester Road, Rochester Hills,
Oakland County, Michigan

Estimated Taxable Value (TV) Increase Rate: 1% per year

Plan Year	0	1	2	3	4	5	6	7	8	9	10	11	12	13
Calendar Year	2021	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035
*Base Taxable Value	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020
Estimated New TV		\$ 2,216,954	\$ 2,239,124	\$ 2,261,515	\$ 2,284,130	\$ 2,306,971	\$ 2,330,041	\$ 2,353,341	\$ 2,376,875	\$ 2,400,644	\$ 2,424,650	\$ 2,448,896	\$ 2,473,385	\$ 2,498,119
Incremental Difference (New TV - Base TV)		\$ 1,599,934	\$ 1,622,104	\$ 1,644,495	\$ 1,667,110	\$ 1,689,951	\$ 1,713,021	\$ 1,736,321	\$ 1,759,855	\$ 1,783,624	\$ 1,807,630	\$ 1,831,876	\$ 1,856,365	\$ 1,881,099

State Millages	Millage Rate	0	1	2	3	4	5	6	7	8	9	10	11	12	13
State Education	6.0000	\$ 3,702	\$ 9,600	\$ 9,733	\$ 9,867	\$ 10,003	\$ 10,140	\$ 10,278	\$ 10,418	\$ 10,559	\$ 10,702	\$ 10,846	\$ 10,991	\$ 11,138	\$ 11,287
Rochester Schools Oper.	18.0000	\$ 11,106	\$ 28,799	\$ 29,198	\$ 29,601	\$ 30,008	\$ 30,419	\$ 30,834	\$ 31,254	\$ 31,677	\$ 32,105	\$ 32,537	\$ 32,974	\$ 33,415	\$ 33,860
School Total	24.0000	\$ 14,808	\$ 38,398	\$ 38,930	\$ 39,468	\$ 40,011	\$ 40,559	\$ 41,113	\$ 41,672	\$ 42,237	\$ 42,807	\$ 43,383	\$ 43,965	\$ 44,553	\$ 45,146

Local Millages	Millage Rate	0	1	2	3	4	5	6	7	8	9	10	11	12	13
Oakland County Operating	4.0200	\$ 2,480	\$ 6,432	\$ 6,521	\$ 6,611	\$ 6,702	\$ 6,794	\$ 6,886	\$ 6,980	\$ 7,075	\$ 7,170	\$ 7,267	\$ 7,364	\$ 7,463	\$ 7,562
Rochester School Sinking	1.4874	\$ 918	\$ 2,380	\$ 2,413	\$ 2,446	\$ 2,480	\$ 2,514	\$ 2,548	\$ 2,583	\$ 2,618	\$ 2,653	\$ 2,689	\$ 2,725	\$ 2,761	\$ 2,798
Oak Int SD-Alloc	0.1918	\$ 118	\$ 307	\$ 311	\$ 315	\$ 320	\$ 324	\$ 329	\$ 333	\$ 338	\$ 342	\$ 347	\$ 351	\$ 356	\$ 361
Oak Int SD-Vtd.	3.0362	\$ 1,873	\$ 4,858	\$ 4,925	\$ 4,993	\$ 5,062	\$ 5,131	\$ 5,201	\$ 5,272	\$ 5,343	\$ 5,415	\$ 5,488	\$ 5,562	\$ 5,636	\$ 5,711
Oak Comm College	1.5184	\$ 937	\$ 2,429	\$ 2,463	\$ 2,497	\$ 2,531	\$ 2,566	\$ 2,601	\$ 2,636	\$ 2,672	\$ 2,708	\$ 2,745	\$ 2,782	\$ 2,819	\$ 2,856
Oakland County Parks	0.3500	\$ 216	\$ 560	\$ 568	\$ 576	\$ 583	\$ 591	\$ 600	\$ 608	\$ 616	\$ 624	\$ 633	\$ 641	\$ 650	\$ 658
Huron Clinton Parks	0.2104	\$ 130	\$ 337	\$ 341	\$ 346	\$ 351	\$ 356	\$ 360	\$ 365	\$ 370	\$ 375	\$ 380	\$ 385	\$ 391	\$ 396
General Fund	2.6909	\$ 1,660	\$ 4,305	\$ 4,365	\$ 4,425	\$ 4,486	\$ 4,547	\$ 4,610	\$ 4,672	\$ 4,736	\$ 4,800	\$ 4,864	\$ 4,929	\$ 4,995	\$ 5,062
Local Street	1.0868	\$ 671	\$ 1,739	\$ 1,763	\$ 1,787	\$ 1,812	\$ 1,837	\$ 1,862	\$ 1,887	\$ 1,913	\$ 1,938	\$ 1,965	\$ 1,991	\$ 2,017	\$ 2,044
Fire Fund	2.7000	\$ 1,666	\$ 4,320	\$ 4,380	\$ 4,440	\$ 4,501	\$ 4,563	\$ 4,625	\$ 4,688	\$ 4,752	\$ 4,816	\$ 4,881	\$ 4,946	\$ 5,012	\$ 5,079
Special Police I	1.1552	\$ 713	\$ 1,848	\$ 1,874	\$ 1,900	\$ 1,926	\$ 1,952	\$ 1,979	\$ 2,006	\$ 2,033	\$ 2,060	\$ 2,088	\$ 2,116	\$ 2,144	\$ 2,173
Special Police II	1.1891	\$ 734	\$ 1,902	\$ 1,929	\$ 1,955	\$ 1,982	\$ 2,010	\$ 2,037	\$ 2,065	\$ 2,093	\$ 2,121	\$ 2,149	\$ 2,178	\$ 2,207	\$ 2,237
Pathway	0.1773	\$ 109	\$ 284	\$ 288	\$ 292	\$ 296	\$ 300	\$ 304	\$ 308	\$ 312	\$ 316	\$ 320	\$ 325	\$ 329	\$ 334
RARA Operating	0.1861	\$ 115	\$ 298	\$ 302	\$ 306	\$ 310	\$ 314	\$ 319	\$ 323	\$ 328	\$ 332	\$ 336	\$ 341	\$ 345	\$ 350
OPC Operating	0.3200	\$ 197	\$ 512	\$ 519	\$ 526	\$ 533	\$ 541	\$ 548	\$ 556	\$ 563	\$ 571	\$ 578	\$ 586	\$ 594	\$ 602
OPC Transportation	0.0954	\$ 59	\$ 153	\$ 155	\$ 157	\$ 159	\$ 161	\$ 163	\$ 166	\$ 168	\$ 170	\$ 172	\$ 175	\$ 177	\$ 179
Library Operating	0.7478	\$ 461	\$ 1,196	\$ 1,213	\$ 1,230	\$ 1,247	\$ 1,264	\$ 1,281	\$ 1,298	\$ 1,316	\$ 1,334	\$ 1,352	\$ 1,370	\$ 1,388	\$ 1,407
Local Total	21.1628	\$ 13,058	\$ 33,859	\$ 34,328	\$ 34,802	\$ 35,281	\$ 35,764	\$ 36,252	\$ 36,745	\$ 37,243	\$ 37,746	\$ 38,255	\$ 38,768	\$ 39,286	\$ 39,809

Non-Capturable Millages	Millage Rate	0	1	2	3	4	5	6	7	8	9	10	11	12	13
Rochester Sch. Debt	3.4800	\$ 2,147	\$ 5,568	\$ 5,645	\$ 5,723	\$ 5,802	\$ 5,881	\$ 5,961	\$ 6,042	\$ 6,124	\$ 6,207	\$ 6,291	\$ 6,375	\$ 6,460	\$ 6,546
Zoo Authority	0.0965	\$ 60	\$ 154	\$ 157	\$ 159	\$ 161	\$ 163	\$ 165	\$ 168	\$ 170	\$ 172	\$ 174	\$ 177	\$ 179	\$ 182
Art Institute	0.1913	\$ 118	\$ 306	\$ 310	\$ 315	\$ 319	\$ 323	\$ 328	\$ 332	\$ 337	\$ 341	\$ 346	\$ 350	\$ 355	\$ 360
Chapter 20 Drain Debt	0.0364	\$ 22	\$ 58	\$ 59	\$ 60	\$ 61	\$ 62	\$ 62	\$ 63	\$ 64	\$ 65	\$ 66	\$ 67	\$ 68	\$ 68
OPC Building Debt	0.1660	\$ 102	\$ 266	\$ 269	\$ 273	\$ 277	\$ 281	\$ 284	\$ 288	\$ 292	\$ 296	\$ 300	\$ 304	\$ 308	\$ 312
Total Non-Capturable Taxes	3.9702	\$ 2,450	\$ 6,352	\$ 6,440	\$ 6,529	\$ 6,619	\$ 6,709	\$ 6,801	\$ 6,894	\$ 6,987	\$ 7,081	\$ 7,177	\$ 7,273	\$ 7,370	\$ 7,468

Total Capturable Millages 45.1628

Total Tax Increment Revenue (TIR) Available for Capture		\$ 33,859	\$ 34,328	\$ 34,802	\$ 35,281	\$ 35,764	\$ 36,252	\$ 36,745	\$ 37,243	\$ 37,746	\$ 38,255	\$ 38,768	\$ 39,286	\$ 39,809
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Table 2
Tax Increment Revenue Capture Estimates
945 and 975 S. Rochester Road, Rochester Hills,
Oakland County, Michigan

Estimated Taxable Value (TV) Increase Rate: 1% per year

Plan Year	0	14	15	16	17	18	19	TOTAL
Calendar Year	2021	2036	2037	2038	2039	2040	2041	
*Base Taxable Value	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	\$ 617,020	
Estimated New TV		\$ 2,523,100	\$ 2,548,331	\$ 2,573,815	\$ 2,599,553	\$ 2,625,548	\$ 2,651,804	
Incremental Difference (New TV - Base TV)		\$ 1,906,080	\$ 1,931,311	\$ 1,956,795	\$ 1,982,533	\$ 2,008,528	\$ 2,034,784	

State Millages	Millage Rate	0	14	15	16	17	18	19	TOTAL
State Education	6.0000	\$ 3,702	\$ 11,436	\$ 11,588	\$ 11,741	\$ 11,895	\$ 12,051	\$ 12,209	\$ 206,481
Rochester Schools Oper.	18.0000	\$ 11,106	\$ 34,309	\$ 34,764	\$ 35,222	\$ 35,686	\$ 36,154	\$ 36,626	\$ 619,442
School Total	24.0000	\$ 14,808	\$ 45,746	\$ 46,351	\$ 46,963	\$ 47,581	\$ 48,205	\$ 48,835	\$ 825,922

Local Millages	Millage Rate	0	14	15	16	17	18	19	TOTAL
Oakland County Operating	4.0200	\$ 2,480	\$ 7,662	\$ 7,764	\$ 7,866	\$ 7,970	\$ 8,074	\$ 8,180	\$ 138,342
Rochester School Sinking	1.4874	\$ 918	\$ 2,835	\$ 2,873	\$ 2,911	\$ 2,949	\$ 2,987	\$ 3,027	\$ 51,187
Oak Int SD-Alloc	0.1918	\$ 118	\$ 366	\$ 370	\$ 375	\$ 380	\$ 385	\$ 390	\$ 6,600
Oak Int SD-Vtd.	3.0362	\$ 1,873	\$ 5,787	\$ 5,864	\$ 5,941	\$ 6,019	\$ 6,098	\$ 6,178	\$ 104,486
Oak Comm College	1.5184	\$ 937	\$ 2,894	\$ 2,933	\$ 2,971	\$ 3,010	\$ 3,050	\$ 3,090	\$ 52,253
Oakland County Parks	0.3500	\$ 216	\$ 667	\$ 676	\$ 685	\$ 694	\$ 703	\$ 712	\$ 12,045
Huron Clinton Parks	0.2104	\$ 130	\$ 401	\$ 406	\$ 412	\$ 417	\$ 423	\$ 428	\$ 7,241
General Fund	2.6909	\$ 1,660	\$ 5,129	\$ 5,197	\$ 5,266	\$ 5,335	\$ 5,405	\$ 5,475	\$ 92,603
Local Street	1.0868	\$ 671	\$ 2,072	\$ 2,099	\$ 2,127	\$ 2,155	\$ 2,183	\$ 2,211	\$ 37,401
Fire Fund	2.7000	\$ 1,666	\$ 5,146	\$ 5,215	\$ 5,283	\$ 5,353	\$ 5,423	\$ 5,494	\$ 92,916
Special Police I	1.1552	\$ 713	\$ 2,202	\$ 2,231	\$ 2,260	\$ 2,290	\$ 2,320	\$ 2,351	\$ 39,754
Special Police II	1.1891	\$ 734	\$ 2,267	\$ 2,297	\$ 2,327	\$ 2,357	\$ 2,388	\$ 2,420	\$ 40,921
Pathway	0.1773	\$ 109	\$ 338	\$ 342	\$ 347	\$ 352	\$ 356	\$ 361	\$ 6,101
RARA Operating	0.1861	\$ 115	\$ 355	\$ 359	\$ 364	\$ 369	\$ 374	\$ 379	\$ 6,404
OPC Operating	0.3200	\$ 197	\$ 610	\$ 618	\$ 626	\$ 634	\$ 643	\$ 651	\$ 11,012
OPC Transportation	0.0954	\$ 59	\$ 182	\$ 184	\$ 187	\$ 189	\$ 192	\$ 194	\$ 3,283
Library Operating	0.7478	\$ 461	\$ 1,425	\$ 1,444	\$ 1,463	\$ 1,483	\$ 1,502	\$ 1,522	\$ 25,734
Local Total	21.1628	\$ 13,058	\$ 40,338	\$ 40,872	\$ 41,411	\$ 41,956	\$ 42,506	\$ 43,062	\$ 728,284

Non-Capturable Millages	Millage Rate	0	14	15	16	17	18	19	TOTAL
Rochester Sch. Debt	3.4800	\$ 2,147	\$ 6,633	\$ 6,721	\$ 6,810	\$ 6,899	\$ 6,990	\$ 7,081	\$ 119,759
Zoo Authority	0.0965	\$ 60	\$ 184	\$ 186	\$ 189	\$ 191	\$ 194	\$ 196	\$ 3,321
Art Institute	0.1913	\$ 118	\$ 365	\$ 369	\$ 374	\$ 379	\$ 384	\$ 389	\$ 6,583
Chapter 20 Drain Debt	0.0364	\$ 22	\$ 69	\$ 70	\$ 71	\$ 72	\$ 73	\$ 74	\$ 1,253
OPC Building Debt	0.1660	\$ 102	\$ 316	\$ 321	\$ 325	\$ 329	\$ 333	\$ 338	\$ 5,713
Total Non-Capturable Taxes	3.9702	\$ 2,450	\$ 7,568	\$ 7,668	\$ 7,769	\$ 7,871	\$ 7,974	\$ 8,078	\$ 136,628

Total Capturable Millages	45.1628								
Total Tax Increment Revenue (TIR) Available for Capture		\$ 40,338	\$ 40,872	\$ 41,411	\$ 41,956	\$ 42,506	\$ 43,062		

Table 3
Tax Increment Reimbursement Estimates
945 and 975 S. Rochester Road, Rochester Hills,
Oakland County, Michigan

Developer Maximum Reimbursement	Total Proportionality	School & Local Taxes	Local-Only Taxes	Total
State	53.14%	\$ -	\$ -	\$ -
Local	46.86%	\$ -	\$ 508,553	\$ 508,553
TOTAL				
EGLE		\$ -	\$ -	\$ -
MSF		\$ -	\$ -	\$ -

Estimated Total Years of Plan: 19

Estimated Capture	
Administrative Fees	\$ 36,414
State Revolving Fund	\$ -
LBRF	\$ 183,318

	1	2	3	4	5	6	7	8	9	10	11	12	13	14
	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036
Total Local Incremental Revenue	\$ 33,859	\$ 34,328	\$ 34,802	\$ 35,281	\$ 35,764	\$ 36,252	\$ 36,745	\$ 37,243	\$ 37,746	\$ 38,255	\$ 38,768	\$ 39,286	\$ 39,809	\$ 40,338
BRA Administrative Fee (5% of Capture)	\$ 1,693	\$ 1,716	\$ 1,740	\$ 1,764	\$ 1,788	\$ 1,813	\$ 1,837	\$ 1,862	\$ 1,887	\$ 1,913	\$ 1,938	\$ 1,964	\$ 1,990	\$ 2,017
BRA RLF Capture (3% of Capture)	\$ 1,016	\$ 1,030	\$ 1,044	\$ 1,058	\$ 1,073	\$ 1,088	\$ 1,102	\$ 1,117	\$ 1,132	\$ 1,148	\$ 1,163	\$ 1,179	\$ 1,194	\$ 1,210
Local TIR Available for Reimbursement	\$ 31,150	\$ 31,582	\$ 32,018	\$ 32,458	\$ 32,903	\$ 33,352	\$ 33,806	\$ 34,264	\$ 34,727	\$ 35,194	\$ 35,666	\$ 36,143	\$ 36,625	\$ 37,111
Total State & Local TIR Available	\$ 31,150	\$ 31,582	\$ 32,018	\$ 32,458	\$ 32,903	\$ 33,352	\$ 33,806	\$ 34,264	\$ 34,727	\$ 35,194	\$ 35,666	\$ 36,143	\$ 36,625	\$ 37,111

DEVELOPER	Beginning Balance	1	2	3	4	5	6	7	8	9	10	11	12	13	14
DEVELOPER Reimbursement Balance	\$ 508,553	\$ 477,402	\$ 445,820	\$ 413,802	\$ 381,344	\$ 348,441	\$ 315,089	\$ 281,283	\$ 247,019	\$ 212,292	\$ 177,098	\$ 141,432	\$ 105,289	\$ 68,664	\$ 31,553

Local Only Costs	\$ 508,553	1	2	3	4	5	6	7	8	9	10	11	12	13	14
Local Tax Reimbursement		\$ 31,150	\$ 31,582	\$ 32,018	\$ 32,458	\$ 32,903	\$ 33,352	\$ 33,806	\$ 34,264	\$ 34,727	\$ 35,194	\$ 35,666	\$ 36,143	\$ 36,625	\$ 37,111
Total Local Only Reimbursement Balance		\$ 477,402	\$ 445,820	\$ 413,802	\$ 381,344	\$ 348,441	\$ 315,089	\$ 281,283	\$ 247,019	\$ 212,292	\$ 177,098	\$ 141,432	\$ 105,289	\$ 68,664	\$ 31,553
Total Annual Developer Reimbursement		\$ 31,150	\$ 31,582	\$ 32,018	\$ 32,458	\$ 32,903	\$ 33,352	\$ 33,806	\$ 34,264	\$ 34,727	\$ 35,194	\$ 35,666	\$ 36,143	\$ 36,625	\$ 37,111

LOCAL BROWNFIELD REVOLVING FUND

LBRF Deposits *		1	2	3	4	5	6	7	8	9	10	11	12	13	14
Local Tax Capture		\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
Total LBRF Capture		\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -

* Per City of Rochester Hills Policy, LBRF capture will take place annually consisting of 3% of available tax increment revenue capture and for five years after eligible activities are reimbursed

Table 3
Tax Increment Reimbursement Estimates
945 and 975 S. Rochester Road, Rochester Hills,
Oakland County, Michigan

	15	16	17	18	19	TOTAL
	2037	2038	2039	2040	2041	
Total Local Incremental Revenue	\$ 40,872	\$ 41,411	\$ 41,956	\$ 42,506	\$ 43,062	\$ 728,284
BRA Administrative Fee (5% of Capture)	\$ 2,044	\$ 2,071	\$ 2,098	\$ 2,125	\$ 2,153	\$ 36,414
BRA RLF Capture (3% of Capture)	\$ 1,226	\$ 1,242	\$ 1,259	\$ 1,275	\$ 1,292	\$ 21,849
Local TIR Available for Reimbursement	\$ 37,602	\$ 38,098	\$ 38,599	\$ 39,106	\$ 39,617	\$ 670,022
Total State & Local TIR Available	\$ 37,602	\$ 38,098	\$ 38,599	\$ 39,106	\$ 39,617	\$ 670,022
DEVELOPER						
<i>DEVELOPER Reimbursement Balance</i>	\$ -	\$ -	\$ -	\$ -	\$ -	
<hr/>						
Local Only Costs						
Local Tax Reimbursement	\$ 31,553					\$ 508,553
Total Local Only Reimbursement Balance	\$ -					\$ -
Total Annual Developer Reimbursement	\$ 31,553	\$ -	\$ -	\$ -	\$ -	\$ 508,553
LOCAL BROWNFIELD REVOLVING FUNI						
LBRF Deposits *						
Local Tax Capture	\$ 6,049	\$ 38,098	\$ 38,599	\$ 39,106	\$ 39,617	\$ 161,469
Total LBRF Capture	\$ 6,049	\$ 38,098	\$ 38,599	\$ 39,106	\$ 39,617	\$ 161,469

* Per City of Rochester Hills Policy, LBRF capture will take place annually consisting of 3% of available tax increment revenue capture and for five years after eligible activities are reimbursed